

# Comprehensive Plan

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## VILLAGE OF --- JOHNSBURG

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## ❖ INTRODUCTION ❖

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The first settlers in the Village of Johnsbury, IL were recorded in 1841. However, with the exception of the Village of Sunnyside, most of the development in the area did not occur within an incorporated municipal jurisdiction until 1992. In that year, the areas which form the majority of the modern Village of Johnsbury were annexed into Sunnyside, and the municipality was renamed the Village of Johnsbury. This annexation resulted from a desire to preserve the quality of life in the area in the face of steady growth. Residents of the area felt that greater control could be exercised if the area was fully incorporated and locally governed.

Shortly thereafter, Village leaders began to create the tools necessary to obtain this control. The 2010 Comprehensive Plan was adopted in February, 1994, and was used as a guide to land use and development decisions in the Village's planning area.

The 2015 Comprehensive Plan was prepared by the Village as an update to that first comprehensive plan, but was never adopted. By 1999, Village officials were aware that the first 2010 Plan was increasingly out of date. An updated plan was needed in order to ensure that the text and the land use plan reflected various land use changes, annexations, and other Village development that had evolved since 1994.

The Planning Committee of the Village was charged with the task of updating the Comprehensive Plan. Through the initial stages of the process, the committee worked with some assistance from a planning consultant.

The Planning Committee began with a series of meetings with the Village Trustees, to discuss suggested land uses for the entire planning area surrounding the Village of Johnsbury. The Committee recognized a remarkable consensus among Village leaders regarding the future vision of Johnsbury. Based on this consensus, the Planning Committee completed a draft future land use plan, including a map and



Historic Downtown Johnsbury

accompanying text. At that point, Village leaders agreed that professional assistance was desirable.

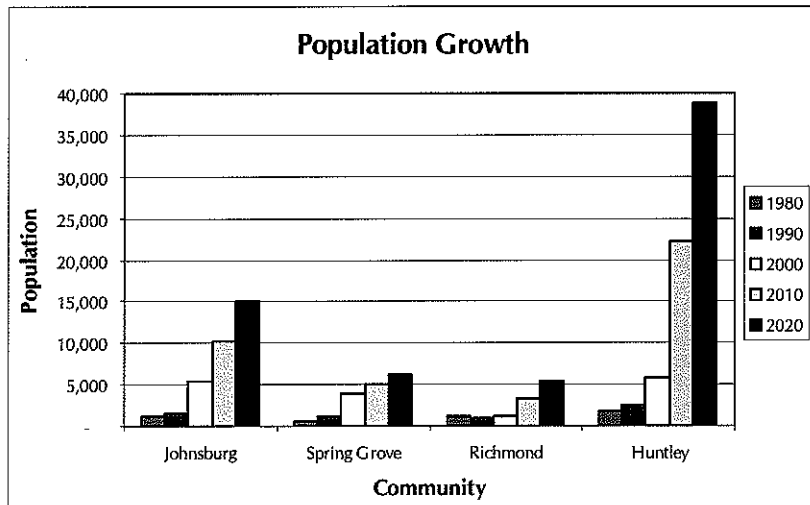
In order to have an updated Future Land Use Plan in place as soon as possible, the Village retained Teska Associates, Inc. to prepare an Interim Comprehensive Plan. The interim Plan provided an up-to-date land use guide that was used by Village officials while the extensive planning process for a thorough Comprehensive Plan was completed.

This Comprehensive Plan builds upon the substantial time and effort that has already been invested by the Planning Committee, and is consistent with previous plans. It recommends future land uses and the transportation network to serve future growth. Finally, a set of Planning Areas divides the planning jurisdiction of the Village into smaller units which are analyzed in more detail.

## ❖ COMMUNITY DEMOGRAPHICS



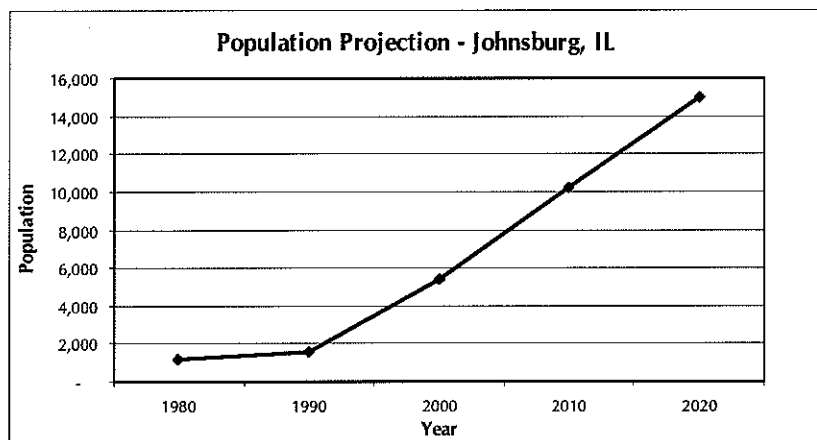
Johnsburg is a relatively small hamlet located in the northeastern portion of McHenry County. It consists of distinctive residential neighborhoods with a limited mix of commercial businesses, industrial establishments, and public and semi-public facilities. Johnsburg's nearest neighbors are Spring Grove, McHenry and Lakemoor. These municipalities are separated by abundant open space. The open space includes farmlands, undeveloped pastures, wetlands and water features.



### Population Growth

The Village of Johnsburg experienced steady population growth prior to 1990. Since 1990, Johnsburg experienced extremely rapid growth. The 2000 census indicated that there were 5,391 residents. This represents a 253% population increase over the decade, or over 25% per year. However, much of the growth in population Johnsburg experienced in 1991 resulted from an annexation agreement with the neighboring Village of Sunnyside. Prior to 1991, the Village of Sunnyside was located in what today is the northeast portion of Johnsburg. Sunnyside annexed the surrounding unincorporated area and then renamed it the Village of Johnsburg. This annexation agreement caused the Village's population to spike to 4,631 in 1991. Since that time, population growth in Johnsburg has been fueled primarily by new single-family residential construction at the periphery of the community. Based on the past trends of growth and the availability of land for development, the population of Johnsburg should continue to grow. The Northeastern Illinois Planning Commission (NIPC) projects that in 2020 the population of Johnsburg will be 15,000 people.

While the historic growth trends in Johnsburg are similar to those seen in comparable communities, Johnsburg showed an increase in population greater than many of these localities. Spring Grove, Huntley and Johnsburg all exhibited rapid population growth between 1990 and 2000. All of the comparable communities are projected by NIPC to grow considerably during the next two decades as development within the Chicagoland area continues to move northwest into McHenry County. Huntley is projected to experience the most rapid growth due to the availability of open land for development, the presence of Sun City and the aggressive development strategy taken by the community.



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## Families and Households

One of the most unique characteristics of Johnsburg's population is its relatively large family size. In 1990, the average household size in the United States was 2.63 persons per household. The Chicagoland area had a household size of 2.72 and McHenry County had a household size of 2.89. Johnsburg's average household size in 1990 was 3.41. While the Johnsburg's household size declined during the last decade to 3.06, it remains higher than the national or regional averages. Johnsburg's unique position is likely due to the large lot sizes and large single-family homes found in the community. These homes are able to accommodate larger families, driving up the household size. Moreover, Johnsburg has a limited amount of multiple-family housing, which further reduces the number of small families.

Another unusual aspect of Johnsburg's population profile is the percentage of persons under 20 years of age. Approximately 34% of the population of Johnsburg is under 20. High levels of persons under 20 years of age is not uncommon many of the fast growing communities in McHenry County. 34% of the population of Algonquin is also under 20, while the percentage is 30% in McHenry. However, this percentage is considerably higher than many communities in the Chicagoland area.

### Population by Age

Age	Persons	Percent
0 to 5	298	6%
5 to 19	1500	28%
20 to 24	230	4%
25 to 34	483	9%
35 to 44	1044	19%
44 to 54	935	17%
55 to 64	490	9%
65 to 74	251	5%
74 to 84	116	2%
85+	44	1%

This demographic shift indicates that an added emphasis should be placed on providing facilities that serve this portion of the population including schools and parks.

## Income

Median income levels within the Village of Johnsburg have increased over the past 10 years. Year 2000 median incomes showed a 42% increase over 1990 median incomes. An increase in income levels may indicate that more households have a larger spendable income to support non-essential commercial establishments such as restaurants, boutique shops, upscale clothing stores, furniture stores, and art stores.

## Housing Stock

The great majority of residential units within the Village of Johnsburg (97%) are single-family units. While the number of multiple-family units has increased slightly since 1990, they still represent less than 3% of the total number of units in Johnsburg. The majority of existing multiple family units are apartment rentals. There are very few condominiums in Johnsburg. Single family attached residences do not represent a significant percentage of the total number of units. The lack of condominium and attached residential units indicates a deficiency in the diversity of the housing stock in Johnsburg and present an opportunity for the Village to increase the housing options available to current and potential residents.

Johnsburg experienced a substantial increase in median home values between 1990 and 2000. During this decade, the median home value in Johnsburg rose from \$121,200 to \$190,900, an increase of about 58%. As noted previously, median income grew only 42% between 1990 and 2000. The result of this disparity is that housing in Johnsburg is becoming less affordable to moderate income families. Housing values in Johnsburg are higher than adjacent communities, with the exceptions of Spring Grove and Ringwood.

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The substantial majority of the residents of these homes moved into Johnsburg during the last ten years. Census information reveals that 57% of all residents moved into the community since 1990 and 83% of residents have moved into the community since 1980.

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## ❖ GOALS AND OBJECTIVES ❖

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The goal of the Village of Johnsburg is to maintain the quality of life that is currently enjoyed. The friendly small town atmosphere and proud heritage that dominates the character and spirit of the Village should be preserved and protected above all else. The Village will accept additional growth in the community which is compatible with the existing rural character of the Village. Existing and future land uses and land use policies should be consistent with this general goal and the specific goals and objectives outlined below.

Goals are presented in bold text for Residential Neighborhoods, Economic Development, Natural Resources, Community Facilities, and Transportation. Specific objectives for each goal are listed in bullet points.

### **RESIDENTIAL NEIGHBORHOODS**

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**Residential neighborhoods with a friendly and harmonious ambiance, including the preservation and enhancement of existing neighborhoods and careful development of new neighborhoods that are compatible with the character of the Village preserve existing pristine land resources and environmentally unique features.**

- ▶ Integrate new neighborhoods into the community at a relatively slow pace to ensure that new growth does not overwhelm the existing quality of life.
- ▶ Impose controls on density and intensity on residential developments to ensure that new neighborhoods are compatible with nearby existing developments.
- ▶ Provide for a balanced range of housing types in appropriate locations that will be compatible with adjacent uses.
- ▶ Maintain a rural character, both physically and visually, particularly along major arterial roads.

### **ECONOMIC DEVELOPMENT**

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**A balance of land uses in the Village, including non-residential uses that serve and employ local residents, and provide a revenue and property tax base for the Village and other local jurisdictions.**

- ▶ Redevelop the Village Center in a historically sensitive manner into a local retail and service area with complementary office development.
- ▶ Encourage the development of commercial nodes with retail uses, followed by office, service and institutional uses, along Spring Grove Road, Johnsburg Road, and Chapel Hill Road.
- ▶ Develop a regional community shopping area and industrial park along the Route 31 corridor.

### **NATURAL RESOURCES**

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**A well-preserved and widely accessible system of natural resources, including the Fox River and its tributaries, to be enjoyed by all residents of Johnsburg.**

- ▶ Encourage the improvement of river water quality.
- ▶ Encourage the protection of groundwater resources.
- ▶ Provide and improve access to the river for use by all residents of Johnsburg as a recreational amenity.
- ▶ Preserve environmentally sensitive areas such as wooded sites, stream corridors, wildlife habitat and water features, under public or private ownership.



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- ▶ Ensure that new developments contribute parks, open space or contributions in lieu to serve new residents.

#### Community Facilities

**A system of public and private community facilities, including schools, recreational facilities, and utilities, which support and enhance the quality of life currently enjoyed by Johnsburg residents.**

- ▶ Support the excellence of the local education system with a controlled growth philosophy and planning for adequate school sites and other facilities to serve existing residents and accommodate new growth.
- ▶ Increase the opportunities for active and passive recreation in the Village by providing sites under public or private ownership.
- ▶ Support continued voluntary sponsorship and user-pay organization models for youth and adult recreational activities with minimal Village financial support.
- ▶ Sponsor entertainment and cultural activities to build community character.
- ▶ Encourage implementation of innovative programs and techniques to enhance septic systems in cooperation with McHenry County Health Department.
- ▶ Provide sewer service to existing medium and high density areas in the Village, particularly adjacent to bodies of water, floodplains, or wetlands.
- ▶ Prepare an overall water system plan to ensure compatibility of the existing private water system, limited public water system, and private wells.
- ▶ Pursue the establishment of intergovernmental boundary agreements with surrounding communities where agreements have not yet been reached.
- ▶ Cooperate with various public districts such as

the township, road district, fire protection district, and County utilizing intergovernmental agreements to their maximum benefit.

#### Transportation

**A transportation network which provides for reliable, safe, and efficient movement of people and goods throughout the community and region, and which anticipates the demands of growth.**

- ▶ Support the acquisition, design and construction of the Richmond Waukegan Corridor, and plan for compatible land uses in this corridor.
- ▶ Encourage the construction of the McHenry by-pass corridor through the Johnsburg area, and continue to plan for right-of-way acquisition and compatible land uses.
- ▶ Encourage safe and efficient traffic circulation with limited congestion, by undertaking road improvements, street extensions, street widening, and improvements to the pedestrian and bicycle networks.
- ▶ Ensure that improvements to Route 31 (a Strategic Regional Arterial) meet the design and construction standards of the Illinois Department of Transportation for the SRA classification, and that development along this roadway includes adequate setbacks, limited curb cuts, and other features compatible with an SRA.
- ▶ Encourage the future consideration of a commuter rail station in or near Johnsburg.

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## ❖ COMPREHENSIVE LAND USE PLAN ❖

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The Comprehensive Land Use Plan is based upon existing and proposed conditions, previous Comprehensive Plans (adopted and drafted), and the recent visioning and planning undertaken by the Village Planning Committee. In addition, the land uses and transportation network depicted on this Plan are intended to implement the Goals and Objectives in the previous section.

The Comprehensive Land Use Plan is a culmination of environmental protection strategies expressed on the Framework Plan. Based on these elements, the Comprehensive Land Use Plan portrays recommendations for future land uses and the transportation network.

### ENVIRONMENT

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The Environment analysis seeks to understand the natural characteristics of the land in order to determine the capacity of the land to support new development (see Environmental Features Map, in the Appendix). Significant natural resources in the study area are identified. These resources should be protected in whole or in part, including woodlands, wetlands, floodplains, rivers, and bodies of water. Poor soils that are unsuitable for development are also identified.

The recognition of these features helps to identify the opportunities and constraints to development. For example, rivers and creeks provide important recreational and aesthetic value, especially for residential uses. However, development should generally not occur within floodplains, due to the increased risk of property and personal damage from flooding. The current Village plans for sewer service, particularly along the Fox River and Pistakee Lake, recognize the importance of protecting water quality in these natural assets.

The natural amenities and development constraints identified as part of the Environment analysis guide the nature and location of land uses and transportation facilities.

### FRAMEWORK PLAN

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Building upon the analysis of the environmental resources, the Framework Plan illustrates the key development strategies for the Johnsburg planning area (see Framework Plan, in the Appendix). The most significant change in relation to previous planning documents is the introduction of a greenway corridor overlaid on environmental resources. These areas should be preserved and protected from development.

The existing and future transportation elements are included on the Framework Plan. The Village has a limited opportunity to develop non-residential uses, which highlights the importance of identifying future locations now. Employment and retail uses are appropriate along major strategic roadways and at intersections. Identifying such locations leads to a hierarchy of non-residential development opportunities.

Four levels of non-residential/commercial areas or "nodes," are generally located at intersections of important roadways and along highly accessible corridors. These development nodes are intended to provide general locations to which the Village can direct its share of regional non-residential development. Level #1 identifies a regional commercial center, including "mass merchandiser" retailers, which has already begun to develop in McHenry. Level #2 represents a sub-regional employment center, including retail, office, or light industrial, which capitalizes on the accessibility of Route 31, Johnsburg Road, and a potential by-pass of McHenry. Level #3 is intended to provide community level shopping, which may include grocery stores and other community services. Finally, several level #4 centers are intended to provide local convenience retailing, such as gas stations, dry cleaners, and convenience stores.

Overall, the Framework Plan is the expression of the fundamental principles which underlie the Comprehensive Land Use Plan.

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## **FUTURE LAND USE**

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The Comprehensive Land Use Plan provides a framework for public and private decisions about how particular sites should develop. It also forms the basis for community facilities and utilities planning. The Comprehensive Land Use Plan is a long-range guide for determining the uses to which the land should ultimately be put, rather than a direct, immediate indication of the appropriate zoning regulations to apply to a specific parcel of land. The purpose of the Comprehensive Land Use Plan is to provide guidelines for controlling the character of the community, ensuring that municipal services and facilities will not be overburdened, and establishing a sound tax base.

The Plan outlines land use suggestions for areas inside the Village limits, along with areas which are within the 1½ mile planning area. Land within this planning area is subject to municipal subdivision regulations, and input from the Village is used in evaluation of any zoning change proposed to the County. These areas may be annexed into the Village of Johnsburg when they become contiguous to the Village limits.

Future land use has not been illustrated on several areas within the Village's 1½ mile planning boundary. The Village has reached a boundary agreement with the City of McHenry, the Village of Spring Grove, and the Village of Ringwood. Therefore, the Plan does not reflect uses in those areas which the Village has agreed are or will be part of the jurisdiction of another municipality.

When reviewing the Plan, it is important to understand the generalized nature and purpose of a land use plan. Given the scale at which municipal comprehensive planning occurs, only broad areas of land use are indicated. Within such areas, certain exceptions may be appropriate.

The Village's entire planning area includes over 8,100 acres. The Land Use Capacity Table (see Appendix), determines the ultimate residential population and non-residential square feet of development that can be accommodated in the

planning area at full build-out.

All recommended residential densities described below are based upon the net buildable acreage of a given parcel of land. The net buildable area of the land is determined by subtracting those portions of the property that are not suitable for development. These areas include flood plains, wetlands, and slopes greater than 20%.

"Cluster" and "conservation" designs for subdivisions are encouraged on parcels with significant natural features, such as wetlands. In cluster or conservation developments, individual residential lots are smaller than a conventional subdivision, so that the remaining land can be held as common open space.

The land use categories on the Comprehensive Land Use Plan are described below.

### **Estate Residential**

This land use category provides for very low density residences, not to exceed 0.5 dwelling units per net buildable acre. This category is applied to those areas located furthest from the older part of Johnsburg, and which are not to be served by public sewer or water. In addition to single family detached residences, compatible educational, religious, and recreation uses also may be permitted in these areas.

Appropriate minimum lot sizes in this category are two acres in conventional subdivisions, or one acre in cluster or conservation developments.

### **Low Density Residential**

This land use category provides for low density residences, not to exceed 1.0 dwelling units per net buildable acre. These areas are intended to provide a sense of visual separation from roadways and nearby development. In addition to single family detached residences, compatible educational, religious, and recreation uses also may be permitted in these areas.

Appropriate minimum lot sizes in this category are one acre in a conventional subdivision, or one half acre in cluster or conservation

developments.

### **Medium Density Residential**

This category provides for residential development in the middle of the range of densities in Johnsborg, not to exceed 3.0 dwelling units per net buildable acre. Areas planned for this classification include existing residential neighborhoods, and areas that are located between low density residential and high density residential or commercial areas. In addition to single family detached residences, compatible educational, religious, and recreation uses also may be permitted in these areas.

Appropriate minimum lot sizes in this category are 10,000 square feet in a conventional subdivision.

Certain medium density residential areas are encouraged to be developed as Planned Unit Developments, to encourage creative site design that preserves site topography and environmental features.

### **High Density Residential**

This category provides for relatively higher density residential development, compatible with the character of established neighborhoods in the Village. Densities shall not exceed 8.0 dwelling units per net buildable acre. This category is applied to established residential developments in the Village. This category also may act as a transition between low density residential and commercial uses.

This category is intended to permit attached housing, to ensure a balanced range of housing types in Johnsborg. Appropriate housing types may include duplexes, townhomes or senior living facilities.

### **Commercial**

This land use category provides for regional and community retail, service and office uses. The areas shown on the Comprehensive Land Use Plan are flexible, although commercial developments should be located at intersections

of arterial roads with other arterials or collector roads. The development of a commercial node should be followed by the development of adjacent areas in complementary uses (such as office, service, or institutional uses).

### **Office/Research**

This land use category provides opportunities for nuisance-free office and research firms, preferably in well-designed, attractive buildings in a heavily landscaped campus-like atmosphere. These uses should have access to arterial roads, to avoid increasing traffic in residential neighborhoods. High tech, engineering, and research uses are particularly appropriate for this category.

### **Mixed Use**

This land use category is designed to provide opportunities for a variety of potential uses, including mixtures of retail, office, and limited institutional development. Where appropriate and where permitted by the terms of the annexation agreements, single- and multiple-family residential may also be provided, particularly near existing public resources. Any residential neighborhoods in mixed use areas should be surrounded by landscaped buffers to minimize impacts from non-residential use. Projects within this category will need to be evaluated on an individual basis consistent with information available at the time of review, and based on potential impact to surrounding properties.

### **Light Industrial/Business Park**

This land use category provides for light industrial and compatible warehouse uses. These uses are primarily located near arterial routes. It is envisioned that most new industrial development will occur within planned business parks in a campus setting.

### **Village Center**

The Village Center category identifies the original core of the Village of Johnsborg. This area has historically served as the primary center for business and civic activity in the community.

This land use category recognizes and strengthens the retail and civic function of the downtown as the focal point of community life. Infill development and rehabilitation and re-use of existing structures will be the primary activities in this area. In addition to its primary civic and retail function, this category also includes mixed uses, which may include medium to high density residential uses which help support downtown businesses. Development in areas designated under this category should be pedestrian oriented.

### **Municipal/Public**

This category identifies existing Village facilities, schools, and other public sites in the Village.

### **Public Open Space and Conservation**

The Public Open Space and Conservation category includes all public open space and recreation facilities such as parks, playgrounds, public tennis courts, bike trails, picnic areas and other recreation facilities.

### **Private Open Space**

This category identifies lands that are sensitive to development, which contain unique environmental characteristics that should be preserved. These characteristics include: wetlands, flood plains, significant tree coverage, and prairies. These areas also provide the Village with such natural functions as flood storage and conveyance, pollution control, and wildlife habitats. Conservation easements, land trusts and other preservation techniques are encouraged to protect these valuable assets of Johnsburg.

### **TRANSPORTATION NETWORK**

The Comprehensive Land Use Plan illustrates existing and proposed components of the transportation system. In addition to serving the function of moving traffic, roadways can also be used to divide land uses of differing type or intensity. The typical Transitional Road Design (see Appendix) illustrates landscaping and buffering techniques that can provide a transition

between a variety of land uses in the undeveloped portions of Johnsburg. Where possible, incompatible land uses on the Comprehensive Land Use Plan are separated by existing or proposed roadways.

The roadway system in Johnsburg is comprised of five components: The Richmond Waukegan Corridor, major arterials, minor arterials, collectors, and local access streets.

The Richmond Waukegan Corridor is a proposed highway along the southern edge of the Village planning area. Although some of the route has not yet been decided due to environmental concerns, the State has acquired significant right-of-way for this planned highway. This route will provide a link between Highway 53 in Lake County and Wisconsin to the north. The Comprehensive Land Use Plan illustrates the potential route for the Richmond Waukegan Corridor, as well as possible interchange locations with Route 31 and Ringwood Road. The Village of Johnsburg supports continuing efforts to acquire, design, and construct this highway, anticipating significant improvements to accessibility and increased attractiveness of the Village for non-residential development. However, the realignment of the intersection of Chapel Hill Road and Johnsburg Road would have detrimental effects on the economic development goals of the Village Center, and therefore is discouraged.

Major arterials are the next classification level, providing primarily regional access and some local access. Route 31 is the most important major arterial in the community. This Route has been designated a Strategic Regional Arterial (SRA) by the Illinois Department of Transportation, and all development along Route 31 should be consistent with this designation. The Village supports the A potential by-pass around the City of McHenry would also be a major arterial, as is Route 12, in the northeast corner of the planning area.

Minor arterials provide access to the larger regional thoroughfare system, with more emphasis on land access and distribution of traffic to a smaller geographic area. Examples in the

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Village of Johnsburg include Johnsburg Road, Chapel Hill Road, Bay Road, Ringwood Road, Miller Road, Spring Grove Road, S. Solon Road, and Hart's/Sunset Road. The Village recommends that Miller Road is extended west to Route 31 and east to Route 12.

Collectors collect traffic from local streets and channel it on to the arterial street system. In Johnsburg and Pioneer Road serve collector functions. There are numerous recommended collectors in the planning area. Collector streets provide additional route choices, particularly for local traffic, to ease the burden on major arterial roads and distribute traffic throughout the Village.

Finally, local access streets are designed to handle low volumes of traffic, and provide access to individual homes and businesses. In general, new subdivisions shall include interconnections with existing local streets in developed areas, and provide for future interconnections to adjacent undeveloped areas.

The transportation element of the Comprehensive Land Use Plan also includes a proposed bike path system. This will provide an alternative means of transportation as well as recreation opportunities. Wherever possible, local trails should link to key community facilities (such as schools and parks) and regional trails, including the McHenry County trail along the railroad tracks southwest of Route 31. Trails should be incorporated into new developments and subdivisions during the development process, and should generally follow the recommendations of the proposed bike path system. Preference should be given to establishing, if feasible, an on-road/off-road bike path along Johnsburg Road. Additionally, a path along Riverside Drive should connect with McHenry.

Finally, a conceptual commuter rail station location is illustrated as part of the transportation network. A branch of Metra's Union Pacific Northwest Line currently terminates in McHenry, and an extension of this line may occur. However, the 2020 Regional Transportation Plan, generated by the Chicago Area Transportation Study, does not contemplate an extension of this line as of yet. As a result, such an extension is unlikely in the next twenty years.

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## ❖ COMMUNITY FACILITIES ❖

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Community facilities include both public and semi-public uses. Facilities such as Village Hall, the proposed sewage treatment plant, Johnsborg High School and the McHenry Township Community Center and Adams' Park are examples of community facilities. The type of growth expected to occur within the next twenty years in and around Johnsborg may overburden community facilities without long range planning and policy analysis. Planning for these functions is an important purpose of the Comprehensive Plan. This section undertakes such planning for parks, open space, and environment; schools; municipal buildings; public works; public safety; libraries; and archeological and historic sites.

### **PARKS, OPEN SPACE AND ENVIRONMENT**

To address park and open space needs in Johnsborg, a four-step framework has been developed. First, goals and objectives are created to guide park and open space planning efforts. These are detailed in the Goals and Objectives section of the Comprehensive Plan, particularly under the headings of Natural Resources and Community Facilities. Next, a series of park types is defined. Each type of park serves a unique function, from provision of neighborhood facilities to specialized parks for a particular sport. Based on these park types, a needs analysis has been developed. This analysis compares Johnsborg's current and projected population with accepted national park acreage standards. The fourth step evaluates alternative management approaches to providing these needed park programs and facilities.

To provide pedestrian and bicycle access to existing and future parks, a system of community trails is outlined. This trail system will also provide a linkage between parks, and promote bicycling as an alternative mode of travel to work, school and recreation.

### **Park Types**

Before acquisition and development of specific park sites and facilities, it is important to establish a framework for park development. Different types of parks fill different community needs. To meet the growing recreational and open space needs of residents of Johnsborg, three general types of parks are identified below. A general description of the purpose and use of each park type is provided, along with a listing of key location considerations, identification of typical facilities, and design issues.

When reviewing the typical facilities list, it is important to note that all parks will not have all facilities listed. For example, all community parks will not have ball diamonds, trails or a water feature. Facilities in a particular park should be based on the appropriateness of a facility to a particular site, and the need for facilities in a particular location.

#### Neighborhood Parks

##### *Description:*

Neighborhood parks provide residents with "close-to-home" open space and recreational facilities. Ideally, all residents would have a neighborhood park within a half mile of their home. However, given Johnsborg's relatively low density of development, a neighborhood park within 3/4 of a mile of every resident is probably a more realistic goal.

The size of each neighborhood park will vary with the size and population of the neighborhood served, but should generally be between 3 and 12 acres. The Dutch Creek Woodlands Park is a good example of a Neighborhood Park in Johnsborg. The following are characteristics of an effective neighborhood park:

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*Location:*

- ▶ Easily accessible to neighborhood residents, including children
- ▶ In residential areas for easy pedestrian access
- ▶ For safety purposes, neighborhood parks should not be located on an arterial roadway
- ▶ Generally within a 1/2 to 3/4 mile radius from all residents served by the park, and centrally located within the neighborhood area
- ▶ Should have physical and visual access from a residential street on at least one side of the park
- ▶ The majority of each park should be located out of the flood plain

*Typical Facilities:*

- ▶ Playground
- ▶ Informal sports fields where conditions permit
- ▶ Multi-purpose court (basketball/volleyball, etc.)
- ▶ Picnic tables and/or shelters
- ▶ Landscape improvements as detailed below

*Design Issues:*

- ▶ Facilities should not be lighted in a neighborhood park to preserve the residential nature of the area surrounding the park. However, security lighting should be included as appropriate for safety and to reduce vandalism
- ▶ Playground areas should be located away from roadways for child safety
- ▶ Landscaping should delineate use areas and provide shade near playground and picnic areas

- ▶ Existing trees should be preserved and enhanced
- ▶ Off-street parking should be provided in locations where on-street parking would be unsafe, or where sports fields will be actively used for practice of team sports

Community Parks

*Description:*

As the name implies, a community park provides facilities and open space for the entire municipality. This type of park serves as a community focal point, providing a site for special events, sports tournaments, and daily recreational enjoyment of Johnsburg residents. Community parks are typically between 30 and 50 acres, although larger sites are common when associated with a nature preserve. Smaller parks are also classified as Community Parks when their function is to provide facilities to the entire community - not just to a specific neighborhood or activity. Adams Park, operated by the McHenry Township Parks Department, is an example of a community park.

*Location:*

- ▶ Easily accessible from all parts of the community, preferably on a collector street, and can be adjacent to an arterial roadway
- ▶ The site should be appropriate for the facilities outlined below without the need for major earthwork
- ▶ Some floodplain area may be associated with the park, but most of the site should be located outside the floodplain

*Typical Facilities:*

- ▶ Lighted sports fields/complex (softball, baseball, soccer, football)
- ▶ Open space for casual active play (softball, football, frisbee, etc.)
- ▶ Picnic tables and group picnic shelters



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- ▶ Hike/bike trails
  - ▶ Landscape improvements as detailed below. Special botanical displays and/or gardens would be appropriate
  - ▶ Area for staging of special events (open area and possible amphitheater)
  - ▶ Playground area
  - ▶ Off-street parking
  - ▶ Tennis courts
  - ▶ Toboggan/sledding hills
  - ▶ Swimming pool/aquatic facility
  - ▶ Passive open space and wooded areas
  - ▶ Public restrooms in high activity areas
  - ▶ Multi-purpose courts for basketball and other activities requiring a paved surface
  - ▶ Water features and/or lake

*Design Issues:*

- ▶ Playground areas should be located away from roadways for child safety
- ▶ Landscaping should delineate use areas and provide shade near playground and picnic areas
- ▶ Site layout should take advantage of natural site characteristics to reduce construction costs and to preserve existing vegetation whenever possible

Open Space/Special Use Parks

*Description:*

Certain facilities, such as open space, special use parks, and wetlands cannot be properly classified as neighborhood or community parks.

Open space areas, while not specifically parks, do play an important role in fulfilling the goal of promoting a healthy relationship between people, open space and the environment.

Open space areas shown on the Community Facilities Plan represent land that is currently used for open recreational use (i.e. a golf course), land that is in a designated 100-year flood plain, or land which possesses some unique natural characteristic. Areas of significant tree cover, or areas of steep slope would fall into this category. Open space areas are intended for preservation and passive recreational uses such as picnicking and trails. These areas could be under private or public ownership.

A prominent example in the Johnsborg area is the McHenry County Conservation District (MCCD) property, near Bald Knob Marina. In addition, a smaller site south of Windy Prairie Acres has recently been dedicated to the MCCD. Both these sites will provide significant open space and natural areas for the enjoyment of residents in Johnsborg and the region. While the MCCD's plans for the newly acquired property are not yet finalized, features could include prairie restoration and a bicycle/pedestrian trail link along the creek.

Finally, special use parks provide employees and residents with a recreational outlet, but are not appropriately classified as neighborhood or community parks. Greenbelt parks and golf courses are both examples of other special use parks. Chapel Hill Country Club is an existing special use park just south of the Village limits.

Wetlands are a special open space feature that have received a great deal of attention in recent years. Wetlands perform many valuable functions, including storm water management, ground water recharge, and the provision of habitats for many types of flora and fauna. Given these environmental benefits, it is desirable to plan for the preservation of existing identified wetlands.

The wetlands shown on the Community Facilities Plan are generalized locations from the National Wetlands Inventory. These areas should be

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protected to the fullest practical extent during the development process to preserve their many environmental benefits. Preservation of wetlands will also provide visual open spaces within developing areas of the community.

*Location:*

- ▶ Convenient to residents served
- ▶ On site with unusual natural amenities, regardless of location
- ▶ Where property provides a connection or link between open spaces, parks, or other public facilities
- ▶ Wherever wetlands larger than one acre exist

*Possible Facilities:*

- ▶ Nature trails
- ▶ Jogging/bicycle trails
- ▶ Nature center or nature preserve
- ▶ Viewing area with interpretive signage
- ▶ Picnic area
- ▶ Major sports complex (softball, soccer, etc.)
- ▶ Public plaza
- ▶ Farmers' market
- ▶ Golf course and/or driving range
- ▶ Aquatic facility
- ▶ Toboggan/sledding hills
- ▶ Other desired public recreational facilities
- ▶ Wetlands typically have no facilities, although nature trails or interpretive facilities may be provided in certain locations associated with other usable parks and facilities

*Design Issues:*

- ▶ Site design should be sensitive to the natural environment, limiting access as needed to preserve fragile areas
- ▶ Any manipulation of an existing jurisdictional wetland, including mitigation and use for storm water detention, should be approved by the Army Corps of Engineers per their 404 permit regulations

**Needs Assessment**

Current and projected park and open space needs within Johnsborg are evaluated in the following tables. Table 1 outlines existing park and open space areas within Johnsborg. Retention or detention ponds in subdivisions are generally not usable park space and have not been included in the amounts shown in Table 1.

Projections of need are based on standards established by the National Park and Recreation Association and are stated in terms of the number of acres per 1,000 people. As noted in the following tables, the National Park and Recreation Association has not established set standards for open space or special use parks. These types of park facilities are highly dependent on the local population and the physical characteristics of a particular community. However, such facilities are considered important to the overall recreational system, and should be provided by the local municipality or park district.

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**Table 1 - Existing Johnsborg Parks and Open Space**

	Total Acres
<b>NEIGHBORHOOD PARKS</b>	
Sunnyside Memorial Park	12.0
Oakcreek Circle Park	7.4
Riverview Drive Parks	0.4
SUBTOTAL	19.8
<b>COMMUNITY PARKS</b>	
Adams Park	52.5
Hiller Park	18.0
SUBTOTAL	70.5
<b>PUBLIC OPEN SPACE/SPECIAL USE PARKS</b>	
Whispering Hills Community Center	0.1
Boat Launch	0.4
Secondary Boat Launch	0.3
Riverview Drive Open Spaces/Beach	0.3
Shiloh Ridge Open Spaces	7.8
Dutch Creek Estates Open Space	64.0
Cherokee Ridge Open Space	20.0
Hickory Way Open Space	3.3
Greenway Path	9.9
Kildor Lane Open Space	1.9
Dakota Ridge Open Spaces	10.5
MCCD Preserve (near Bald Knob)	51.4
MCCD Preserve (south of Windy Prairie)	19.1
SUBTOTAL	189.0
<b>PRIVATE OPEN SPACE/SPECIAL USE PARKS</b>	
Hunt Club	47.0
SUBTOTAL	47.0
TOTAL SYSTEM	326.3

**Table 2 - Comparison of Standards to Developed Park Land - Current**

Park Type	Standard (Acres per 1,000 Population)	Standard Requirement (acres)	2001 Existing (acres)	Surplus/ (Deficit)
Neighborhood Parks	3.5	18.9	19.8	0.9
Community Parks	6.5	35.0	70.5	35.5
Public Open Space /Special Use Parks	n/a	n/a	189.0	189.0
Private Open Space /Special Use Parks	n/a	n/a	47.0	47.0
<b>Total</b>	<b>10.0</b>	<b>53.9</b>	<b>326.3</b>	<b>272.4</b>

**Table 3 - Comparison of Standards to Developed Park Land - Village Build-Out**

Park Type	Standard per 1,000 Population	Standard (Acres per 1,000 Population)	2001 Existing (acres)	Surplus/ (Deficit)
Neighborhood Parks	3.5	29.8	19.8	(10.1)
Community Parks	6.5	55.4	70.5	15.1
Public Open Space /Special Use Parks	n/a	n/a	189.0	189.0
Private Open Space /Special Use Parks	n/a	n/a	47.0	47.0
<b>Total</b>	<b>10.0</b>	<b>166.1</b>	<b>326.3</b>	<b>160.2</b>

Table 2 shows that Johnsborg currently meets national standards for community and neighborhood park use, based on the 2000 population of 5,391. In addition, many subdivisions have open space and special use parks to further provide open space and recreational amenities to Johnsborg residents.

Table 3 illustrates that even more parkland will be required as the Village grows. In this table, an estimate of additional park land requirements was determined by comparing park standards to an estimated future population of 8,525 residents. This population estimate assumes ultimate build-out in accordance with the Future Land Use Plan and densities at the *low range*.

As Table 3 indicates, in time the Village will face a deficit in neighborhood parks. Johnsborg should reserve neighborhood parks land in all new subdivisions, through the application of park

dedication requirements in the subdivision process and careful review of proposed new residential developments. Such park sites may function as undeveloped open space until funds become available to develop them as park sites with specific recreational facilities. The identification of park sites within each subdivision should be based upon the location criteria on page 11.

Although neighborhood parks can also be accomplished through the cash dedication process, this will require additional effort on the part of the Village to identify and acquire appropriate parcels for development as parks. To ensure a fair price for property, negotiations for property acquisition will need to be kept private until a price agreement is reached with a willing seller. If a negotiated price cannot be reached, the Village does have the authority to acquire property through condemnation. However, the added legal cost of this approach suggests that condemnation should

only be used as a last resort on property ideally located and configured for park purposes.

In the future, the demand for community park land will also grow. One of the most logical ways to address this situation would be to improve the facilities at Hiller Park. In addition, *if residential densities are in the mid to high range* as contemplated by this plan, potential expansion of Hiller Park would be reasonable. This will provide a contiguous site that can accommodate a variety of recreational programs and needs of Johnsborg residents in a single location. In addition, Hiller Park is located adjacent to the high school, which suggests the possibility of shared facilities.

If expansion of Hiller Park is either not possible or insufficient, then other options for park space should be explored. The Community Facilities Plan indicates a **conceptual** location for a future township or community park. This potential location can take advantage of proximity to the new McHenry County Conservation District acquisition and the nearby floodplain/open space corridor. In addition, this area is relatively central in the planning area, making it accessible to all residents (particularly if the trail network is fully developed as recommended herein). The actual location of a new community park will depend upon the characteristics of the site (including the location criteria on page 11) and negotiations with property owners.

The Village should seek the expansion of McHenry Township's Adams Park. If this is not feasible, the Village should add a new community park adjacent to Adams Park.

### **Park and Recreation Management**

Currently, the McHenry Township Park Department operates two facilities in the Johnsborg planning area: Adams' Park and the McHenry Township Recreation Center; and the Whispering Hills Community Center. The Village of Johnsborg has recently begun operating a very limited recreation program, which is fully supported by user fees. Other recreation programs are operated by volunteer based organizations such as Little League. Village parks are maintained by the Village Public Works Department.

As the population of Johnsborg continues to grow, demand for park facilities and programs will also increase. In addition, the Village may also begin to experience more demand for programs based on the types of residents. For example, more families with children may begin to occupy new multi-family housing or older, starter-size homes (as older residents sell). Regional trends suggest that new residents often demand a higher level of service than their precursors, as they have moved from closer suburban areas where services are more prevalent.

Therefore, although the status quo can function effectively in the short run, long range planning is necessary to ensure effective on-going provision of desired services. Four alternatives can provide for future management of parks and recreation in Johnsborg.

First, the Village could create a limited Parks Department, which could continue to be funded by user fees to the greatest extent possible. In addition to the limited recreation programs now offered by the Village, this department may take over park maintenance from the Public Works department. Such a department might be a temporary stepping stone to a Park District or larger park and recreation department.

Second, the Village could establish a permanent Parks and Recreation Department, including establishment of a full time staff and construction of recreational facilities. In this case, overlap with the facilities of the McHenry Township Park Department should be avoided, to ensure that residents are not taxed twice for similar services.

Third, assuming full build out in Johnsborg at maximum densities, the total population could support an entirely new park district. A Johnsborg Park District would have a focused objective of providing quality facilities and recreational programs, without having to balance demands for other municipal services against a very limited Village tax levy. To create a new park district, voter approval is required under state statutes. This demands strong community support and a citizen-initiated petition for a referendum. The Village itself would not be involved in the creation of a park district.

Finally, without support to create a new Park District, the Village would continue to be the provider of park and recreational opportunities in Johnsburg. This option involves encouraging private enterprise, such as local horse farms and health clubs, to provide services, possibly in partnership with the Village. Cooperative agreements with the local School District and McHenry Township could promote joint use of facilities and provide increased access to recreational programs. The provision of private recreational facilities or the establishment of special service areas should also be explored to help develop, program, and maintain needed park and recreational facilities within the community.

Several state and federal grant programs are available to assist communities such as Johnsburg in the development of a local park and open space system. Johnsburg should take advantage of these programs to stretch limited local financial resources while providing residents with needed recreational facilities.

## **Trails**

### *Description:*

The Open Space Plan outlines an extensive new trail system. This system provides opportunities for recreation and for transportation to key employment centers and key community facilities, such as parks and schools. Through development of the trail system, Johnsburg can provide residents with safe pedestrian and bicycle access to community facilities while reducing dependency on the automobile as the only viable transportation option.

The existing trail opportunity in Johnsburg is found along the Metra right-of-way in the southwestern portion of the planning area. This trail forms part of the McHenry County Prairie Trail and the Grand Illinois Trail, a 475-mile loop through northern Illinois.

The Open Space Plan shows numerous recommendations for additional trail alignments. Highlights of the proposed network of trails in Johnsburg include:

- ▶ Major connections in floodplain/greenways and

along the north side of the Fox River.

- ▶ Construction of paths along proposed collectors throughout the Village.
- ▶ Links to the Prairie Trail/Grand Illinois Trail.

### *Location:*

- ▶ Convenient to residents served
- ▶ Coordinated with existing or proposed trails in neighboring municipalities and throughout McHenry County

### *Possible Facilities:*

- ▶ Jogging/bicycle trails
- ▶ Support facilities such as parking, benches, drinking fountains, and trail signs

### *Design Issues:*

- ▶ Site design should be sensitive to the natural environment, routing trails to avoid existing trees and to maximize views of adjacent natural landscapes
- ▶ Coordination of trail planning and design with other municipalities, park districts, MCCD, and the county is necessary to prevent conflicting plans and to achieve consistent standards

## **SCHOOLS**

Currently, the Village of Johnsburg and the Village of Ringwood are served by the Johnsburg Community School District 12. District 12 operates four schools: Ringwood School Primary Center (in Ringwood), James C. Bush Elementary, Johnsburg Junior High, and Johnsburg High School (all in Johnsburg). St. John the Baptist Catholic School is also located in Johnsburg, and provides parochial education in kindergarten through eighth grade.

District 12 currently operates consolidated schools. That is, all students in the district in grades kindergarten through 2 are housed at Ringwood School Primary Center, and all students in grades 3

and 4 are taught at Bush Elementary. In addition, the District prefers a campus approach, exemplified by the adjacent sites of Bush Elementary and Johnsborg Junior High (in Johnsborg's Village Center).

Due to a recent restructuring, the District facilities can accommodate modest growth in students over the next five years without new construction. Further, portable classrooms are likely to be used prior to any additional construction. If new construction is necessary in the future, District 12 prefers to expand existing facilities rather than building at completely new sites (for example, the District has recently purchased 8 acres adjacent to Johnsborg High School for future expansion).

Student population is forecast for the ultimate build-out based on land uses on the Future Land Use Plan. Even assuming densities at the low range, new development may increase the student population in Johnsborg considerably, from 2,412 pupils (in the 2000-2001 school year), to 5,211 students. Of these, 1,666 will be elementary school aged, 1,007 will be junior high school aged, and 2,538 will be high school aged. The addition of these new students will require District 12 to construct new capital facilities.

It is School District responsibility to do capital facility planning. Specific decisions with regard to actual sites and facilities to be acquired for new schools should be based on a more detailed evaluation by the School District. The Village should work closely with all the local school districts in review of development proposals to determine when and where new school sites will be needed. Such planning should continue the established cooperation between District 12 and the Village. New school sites should be located adjacent to neighborhood park sites to the greatest extent possible.

All Johnsborg residents are served by McHenry County Community College District #528.

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#### **MUNICIPAL BUILDINGS**

The Village Hall is currently located in the eastern portion of the Village, adjacent to Sunnyside

Memorial Park. This building is currently operating at full capacity, with no room to accommodate future growth. As the need for additional staff, public meeting space, and records storage grows with the population, additional space will become necessary.

The municipal site is located in a highly accessible location along Route 31, adjacent to the planned wastewater treatment facility. The Public Works department is currently headquartered in a building on this site. Future plans call for this site to be occupied by existing Public Works as well as future staff assigned to sewer and water functions. This site is highly accessible and of ample size to accommodate growth of these functions.

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#### **PUBLIC WORKS**

##### **Water Systems**

The vast majority of the Village and the planning area are served by individual private wells or private water system. A private water system, centered in the older area of Johnsborg, is operated and maintained by a private water/wastewater utility company. The Village is not involved in the funding or operation of this system at the present time. Private expansion of the system to serve adjacent areas is possible, although the Village does not intend for the areas around Spring Grove Road to be served with private or public water systems.

There is also a small municipal water system in Johnsborg, in the Shiloh Ridge and Claremont Hills neighborhoods. This municipal water system will be expanded to provide a 100 GPM shallow well with two 10,000 gallons at grade storage tanks and 6"-8" water mains to serve recently annexed properties lying southwest of Route 31. Most other new residential development in the planning area should be served by private wells or private water systems within each subdivision.

Another municipal water system is currently planned to provide service to the area around Route 31. The first phase of the Route 31 water system will be served by a 400 GPM deep well with a 12" water main extending from the Claremont Hills water system south to the Blake property to serve

the business/commercial corridor along Route 31. A second phase will expand the system for fire protection purposes by providing a 700-10,000 GPM deep well with a 500,000 gallon elevated water storage tank.

The service area located west of the railroad right-of-way to the Village limits is currently planned to provide a water system serviced by two 400-500 GPM wells with a 400,000 gallon elevated water storage tank. The system will also be serviced by a 8" and 12" looped distribution system for domestic flow/fire protection. As development occurs, the system will be enhanced with additional storage, wells, and looping.

### **Sewer System**

At the present time, all properties in the Village are served by private septic fields. However, to accommodate future growth in the southwest sector of the planning area, and to protect water quality in light of the many waterfront homes in the Village, a sanitary system has become necessary.

As a result, the Village has completed final engineering plans for a sanitary sewer system and wastewater treatment plant. As funding becomes available, the Village will extend sewer lines in a phased approach over 7 to 10 years. The first phase of construction of the waste water treatment facility will be completed by July 2003. The areas to be serviced are the higher density lots along the Fox River and the adjacent downtown areas (i.e. schools, library, businesses, churches, etc.). The lots that are 3/4-acres and larger will not be serviced by the collection system. The high school, subdivisions north of the downtown area, and Pistakee Highlands could be serviced after the previously mentioned areas between years 10-20. To maintain relatively low densities and a rural character, the undeveloped lands in the north and west portions of the planning area are not intended to receive sewer service (see Sewer Service Phasing Plan).

The wastewater treatment facility is located on the municipal site on the east side of Route 31, south of Johnsburg Road. This facility will be sized to accommodate waste water from all areas where service is currently planned.



New Sewer Plant Groundbreaking

### **Public Works Activities**

The Public Works Department is currently responsible for maintenance of streets and rights-of-way, municipal buildings and grounds, and some limited utilities maintenance. Examples of streets and rights-of-way maintenance include snow plowing; street cleaning; ditch, culvert, and other stormwater conveyance repair; tree trimming in parkways, medians, and rights-of-way; ditch mowing; pavement patching; street sign and regulatory sign upkeep; and similar functions. Major street resurfacing and rehabilitation are performed by outside contractors at the Village's direction.

Examples of buildings and grounds maintenance include mowing; landscaping and trimming; building maintenance and repair tasks; parking lot and sidewalk snow removal; equipment and supply procurement and management; and similar functions.

Examples of current utilities maintenance include the care of the municipal water system, which consists of water supply production, treatment, storage, testing, and distribution; stormwater systems, which may consist of detention/retention ponds (if maintenance is accepted by or property deeded to the Village), piping, drainage ditches, and other facilities; and perhaps features such as wetlands or waterways associated with stormwater management. Virtually all engineering activities for streets and utility systems are performed by outside



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consultants at the Village's direction.

Refuse, recycling, and yard waste collection in the community is performed by a refuse hauler under contract to the Village.

Future growth and development, and the construction of the sanitary sewer system, will expand the scope and sophistication of work for the Public Works Department. Although initial operation and maintenance of the sewer system will be undertaken by outside contractors, the Village may eventually hire a Public Works Director with a sewer and water license, bringing such functions in-house.

The demands of additional Public Works functions will require additional employees, equipment, and resources to continue to provide adequate and expanded services to the community. Planning for growth as it relates to public works functions will take many forms. Engineering studies of the water and sewer systems should be on-going, to project and plan for capacity, improvements, locations of facilities, capital costs, and other related issues. Pavement management studies along with planning and implementation of an ongoing multi-year street repair program are all necessary in order to protect the Village's investment in public roadways. Other services and facilities, such as buildings and grounds maintenance as well as equipment and vehicle acquisition, must be addressed through a comprehensive and ongoing capital improvements program and budget.

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#### **PUBLIC SAFETY**

Fire protection in Johnsburg and its planning area is provided by the McHenry Township Fire Protection District. The Department has three existing Fire Stations, one of which is located in Johnsburg near the Village Hall. Ambulance service is also provided by the McHenry Township Fire Protection District from the same facilities.

The Johnsburg Fire Station is approximately 11 years old, and is currently staffed by three staff members at a time, on a 24-hours/day basis. The FPD does not anticipate any future stations in the

Johnsburg area, or expansion of the existing stations.

At the present time, the Fire Protection District is staffed on a paid on-call system. However, growth and development within the entire service area may eventually require the change to a system of full-time employment.

The Police Department currently resides in the former Johnsburg Rescue Squad building on Chapel Hill Road. This central, downtown location serves the department well. Not only is this location accessible and convenient, but it contributes to the sense of place and community focus in the downtown.

However, the building itself currently requires maintenance and remodeling. A more efficient layout is needed, particularly through the creation of a central, secure area to receive prisoners. Further, as the Village grows, the building will become inadequate to serve the corresponding growth in the police force.

In the future, additional space could be created by adding a second floor to the existing Rescue Squad building. However, in the long run, it is likely that the current building will not be large enough to accommodate the necessary growth in personnel and equipment. A new facility may therefore be necessary. A downtown location is likely to continue to be preferred, given the advantages of accessibility and community identity.

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#### **LIBRARY**

Residents of the Village of Johnsburg planning area are served by the Johnsburg Public Library District, which operates within the same boundaries as the Johnsburg School District.

The Johnsburg Library is located on Johnsburg Road and is approximately five years of age. The building was designed and constructed to allow for future expansion. Already, the current facilities are reaching capacity, particularly in the adult book section. As a result, the Library Board is anticipating expansion of the Library building within the next five years. Expansion can be

accommodated on the existing site, and new facilities or branch libraries are not anticipated.

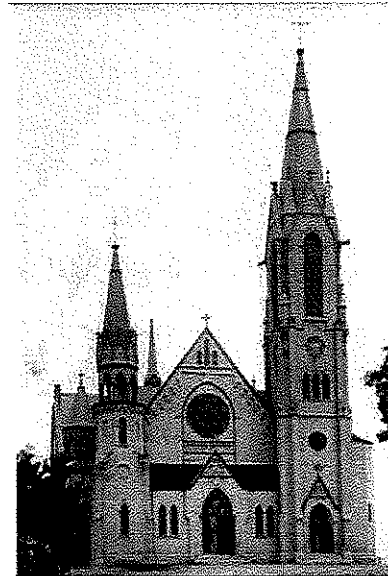
#### **ARCHEOLOGICAL AND HISTORIC SITES**

Although no archeological sites have been identified in Johnsborg to date, the characteristics of the area would have been conducive to early hunting and gathering activities, and such sites may exist. Volunteer efforts may locate such sites, preferably prior to development applications. The Sauk Trail chapter of the Illinois Association for the Advancement of Archeology can provide assistance to landowners who desire it. Sites can be preserved through private easements, or by locating private or public open spaces around such sites.

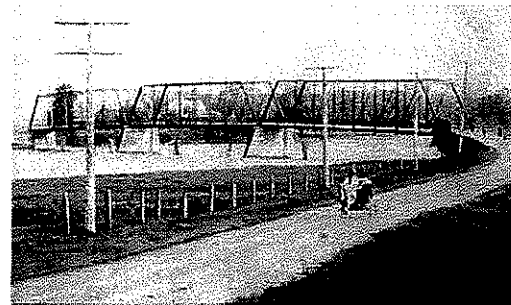
Johnsborg does not yet have a local agency responsible for surveying or designation of local historic resources. Therefore, the Village itself holds the power to designate historic resources which satisfy one or more of the following criteria:

- ▶ The site is associated with an activity or event uniquely associated with Johnsborg, or the area in general if the activity or event occurred prior to Johnsborg's incorporation;
- ▶ The site is associated with a person of noteworthy accomplishment;
- ▶ The site presents a prior culture or activity which significantly influenced the landscape or people of the Johnsborg area.

At present, the church at the intersection of Johnsborg Road and Ringwood Road has been identified as a historic site within the Village of Johnsborg. Additional historic churches include St. John the Baptist Catholic Church and the Schmitt Chapel located on the grounds of the Chapel Hill Country Club.



St. John's Church



Early photograph of Johnsborg

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## ❖ PLANNING AREAS ❖

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Covering over 8,100 acres, Johnsborg's Planning Area and Comprehensive Land Use Plan provides only a generalized picture of desired development patterns. To provide a more detailed evaluation, the community is divided into 10 planning areas. These smaller planning areas share characteristics that make them unique and distinguish them from other areas. In general, major roadways and natural features determine planning area boundaries.

For each planning area issues of critical concern are addressed and development opportunities are identified. The issues addressed in each planning area include the following:

### Existing Land Use

Description of existing conditions and characteristics of the natural and built environment.

### Transportation

Description of existing roadway network and recommendations for future vehicular, bicycle and pedestrian improvements.

### Open Space and Environment

Description of existing environmental and open space amenities, and recommendations for future public open space opportunities and the conservation of environmentally sensitive features.

### Public Facilities

Where information is available, a description of the existing utilities and public facilities available to the area and recommendations for future improvements that may be required to support future development. A complete analysis of Public Facilities has not yet been undertaken, and so this description may not be available for all planning areas at this time.

### Future Land Use

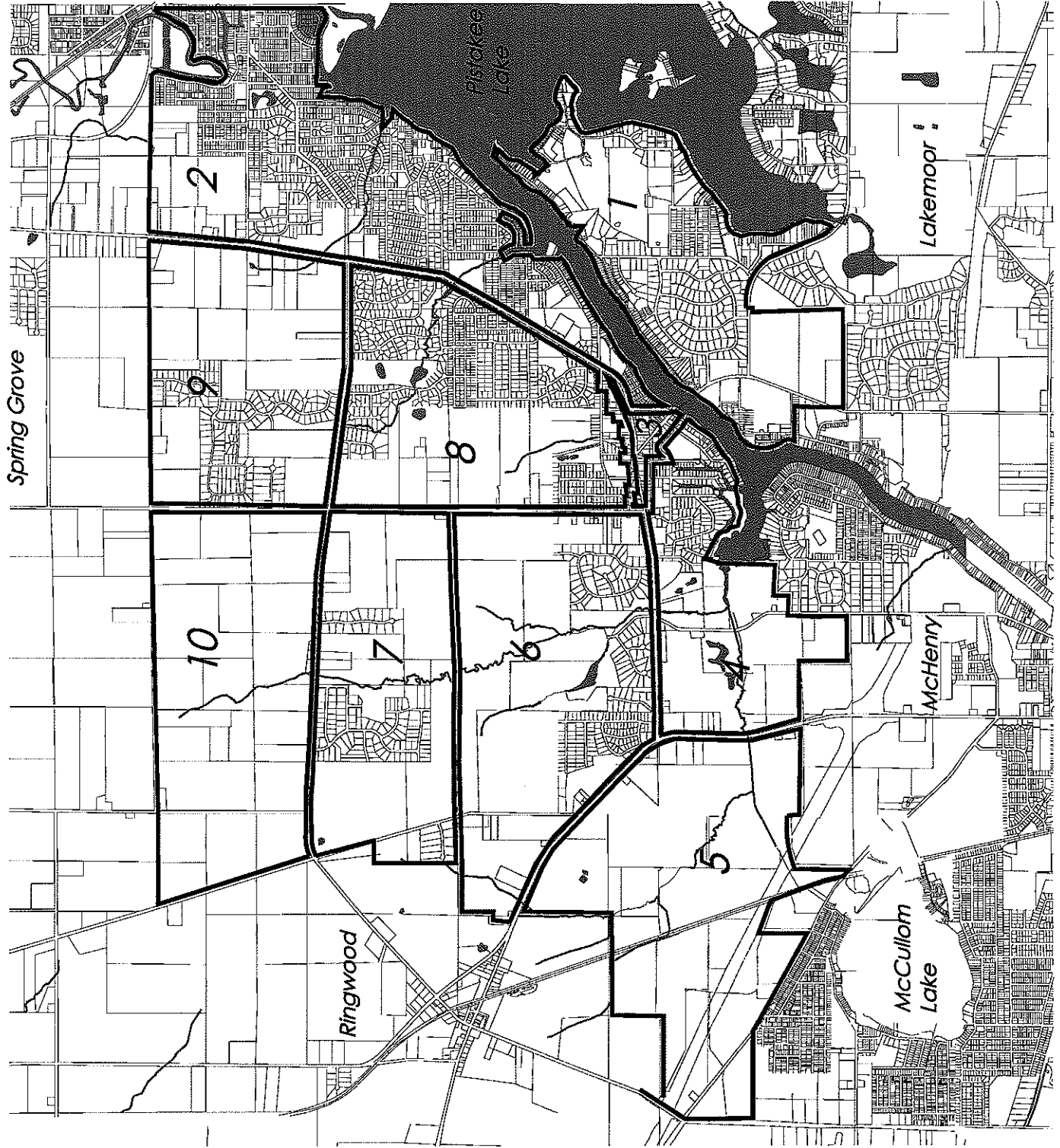
Identification of appropriate future land uses and development patterns.

### Focus

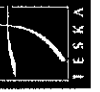
Recommendation for the focus of short-term planning efforts.

Planning Area Boundaries Map  
Village of Johnsburg

— Planning Area Boundaries



December 2003



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## **Planning Area 1**

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Planning Area 1 contains mostly developed residential areas, with some commercial uses on the eastern shore and along Chapel Hill Road. The planning area contains approximately 870 acres.

### **Boundaries**

North: Fox River  
East: Pistakee Lake  
South: McHenry Border agreement line  
West: Fox River

### **Existing Land Use**

This planning area forms the present southeast extent of the Village. The majority of this area is devoted to single family homes, including the one-acre estate subdivision Chapel Hill Estates. Many homes in this planning area front on either the Fox River or Pistakee Bay. Over the past 25 years, many of these homes have transformed from summer cottages to year round full time residences. As these homes have been remodeled, increased property values have resulted.

In addition to homes along the shorelines, there are a small number of commercial uses that cater to recreational needs, such as a yacht club, a marina, and restaurants.

In the northeastern portion of this planning area, there remains a largely undeveloped area. The McHenry County Conservation District owns a large portion of this open tract, and the remainder is owned by a sports club.

Chapel Hill Road runs through the western portion of this planning area, and is fronted by largely commercial uses and vacant lots. North of Bay Road are restaurants, a bowling alley, a motel, other commercial uses, and a number of undeveloped commercial parcels.

South of Bay Road, the planning area is largely agricultural, except for a communication equipment sales and service facility and a golf driving range at the southwest corner.

## **Transportation**

Currently, Chapel Hill Road and Bay Road represent the primary points of ingress and egress to and from the Village of Johnsburg. With the exception of the crossings in McHenry, Chapel Hill Road is the only road to cross the Fox River in the northern part of the County. Recent improvements have been made to Bay Road, and the East McHenry By-Pass, which connects to Chapel Hill Road, was also recently opened.

Due to these improvements and as development progresses in the area, significant increases in traffic are anticipated (particularly along Chapel Hill Road). To provide a north-south alternative for local traffic, a new collector is recommended for this planning area. This proposed collector is located east of Chapel Hill Road, connecting Ramsgate Lane to Long Street.

South of the bridge, direct access from properties to Chapel Hill Road should be limited, and easements for cross access established.

### **Open Space and Environment**

As described above, this planning area includes a sports club and an area owned by the McHenry County Conservation District. These facilities both provide valuable recreational opportunities and environmental amenities and should be preserved.

This planning area is bounded on three sides by the Fox River and Pistakee Bay, and many homes are built within the floodplain areas of these waterways. Any new or expanded construction in this area should be carefully scrutinized to minimize the risk of flooding.

South of Bay Road, a large wetland is included in the undeveloped southeast corner of the planning area. To ensure the preservation of this resource, conservation design techniques are encouraged for any residential development of this site. In a conservation subdivision, the same number of home sites as a conventional subdivision are created. However, each home lot may be smaller than the normal requirements, and the excess land, including natural amenities such as

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wetlands, is preserved as common open space.

### **Public Facilities**

The Village is currently planning a new sewer system to serve existing high density areas of the Village. Medium density residential neighborhoods in this planning area fall within the initial sewer service area. Properties along the Fox River and Pistakee Lake are particularly important, to prevent wastewater run-off into these bodies of water.

### **Future Land Use**

#### Low Density Residential

The center of this planning area contains the low density Chapel Hill Estates subdivision. There are also low density residences along the southeast edge of this planning area. This density is also recommended for future development of the east half of the undeveloped parcel on the south side of Bay Road. Low density development in this location will be compatible with the adjacent properties to the south and east. As described under the Open Space and Environment discussion above, cluster or conservation design subdivisions are encouraged.

#### Medium Density Residential

This category is applied to the existing homes along the Fox River and Pistakee Bay and the older neighborhood in the center of this planning area. The character of these areas, which originally consisted largely of summer cottages, should be preserved as a historical resource of the Village. Due to rising property values of lots along the shoreline, as well as numerous small and functionally obsolete homes, there may be increasing numbers of tear-downs or substantial additions/remodelings. The Village should consider adoption of some form of floor-area and/or ground-coverage ratios in order to generally preserve the scale and established character of the neighborhood. In addition, many of the homes in this category are located within floodplain areas, and precautions should be taken if new or expanded development is contemplated.

#### Commercial

Existing commercial uses on Oakgrove Road and along Bald Knob Road, in the most easterly part of the planning area, are designated for continuing commercial use. Although specific locations are not shown on the Comprehensive Land Use Plan, the Village may also consider appropriate new or expanded commercial uses along the waterfront. Small scale and/or seasonal commercial uses would be considered on a case-by-case basis and must demonstrate compatibility with surrounding residential uses.

Commercial uses are also anticipated to be maintained and further developed on either side of Chapel Hill Road, north of Bay Road. New developments should be subject to the following design guidelines:

1. All structures should be internally oriented with delivery access at the rear.
2. All structures should be designed according to a consistent architectural theme.
3. Parking areas should include landscaped islands.
4. Landscaping shall be used to screen the buildings and activities from adjacent uses and roadways.
5. Site lighting should be of low luminescence so as to have no adverse affect on adjacent uses.
6. Each use should have no more than one point of ingress/egress to Chapel Hill Road, and only one such point to Bay Road, where applicable. Where feasible, interconnections between adjacent uses should be encouraged.
7. Development should include right-of-way dedications of adequate width to accommodate the future widening of both Chapel Hill Road and Bay Road. In addition, adequate roadway improvements at property entrances must be constructed by developers.

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8. No roof signage should be allowed.

#### **Focus**

9. All refuse disposal containers should be housed within the structures or completely screened from view from public rights-of-way.

Planning efforts for this area should be careful development along Chapel Hill Road to minimize traffic conflicts and establishes a high quality image along this primary gateway into Johnsburg.

Commercial development should generally be of the same quality of site and architectural design as the most recent commercial development in this planning area.

#### Office/Research

This category is applied to the properties on either side of Chapel Hill Road, south of Bay Road. These uses can include low intensity office and research facilities in an attractive setting. In addition to the site design guidelines described in the Commercial category, new office/research developments should be subject to the following:

1. All structures should be no taller than two stories to minimize the impact on existing residential neighborhoods.
2. Landscaped earthen berms should be used to screen the buildings and activities from adjacent uses and roadways.
3. All exterior perimeter site signage should be limited to directory signs at each point of entry.

#### Open Space and Conservation

This designation is applied to the properties currently owned by the McHenry County Conservation District and a local sports club. Due to the environmental sensitivity of these properties and their amenity value for local residents, the current open space and conservation uses should be maintained.

Private Open Space and Conservation is also recommended for the wetland area in the undeveloped southeast corner of the planning area. This wetland should be incorporated into common open space or dedicated to a conservation organization, for permanent preservation.

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## **PLANNING AREA 2**

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Planning Area 2 contains mostly older, developed residential neighborhoods of medium density. Most of these neighborhoods are not currently incorporated within Village boundaries. The planning area contains approximately 1,190 acres.

### **Boundaries**

North: Miller Road and Route 12  
East: Fox River, Pistakee Lake, and the Village Planning Area boundary  
South: Fox River and Pistakee Bay  
West: Fairview Avenue and Johnsbury Road

### **Existing Land Use**

This planning area, lying northwesterly of the Fox River, is largely composed of developed residential neighborhoods. For the most part, these neighborhoods have been developed at a medium density (in particular, to McHenry County standards). There are also a small number of varied commercial uses along Johnsbury Road, along with the current Village Hall and park.

East of Johnsbury Road and north of May Avenue, the majority of the planning area is not incorporated within the boundaries of the Village of Johnsbury. However, residents of this area strongly identify with Johnsbury, and it is conceivable that these areas could be annexed into Johnsbury.

### **Transportation**

This planning area is served by several nearby major and minor arterials. Johnsbury Road forms the western boundary of the planning area. Ringwood Road traverses through the planning area in an southwesterly-northeasterly direction, through developed neighborhoods. Route 12, one of only two major arterials in the Johnsbury area, is located just northeast of this planning area.

Recommended improvements to the transportation network in this planning area include the extension of Miller Road east to Route 12. The function and visibility of Ringwood Road as a minor arterial should be strengthened. Finally, new subdivisions in undeveloped portions of this planning area should extend the existing street network of adjacent subdivisions.

The Future Land Use section of this planning area description includes design guidelines applicable to commercial development. Many of these guidelines are concerned with the efficient flow of traffic, particularly in maintaining the arterial function of Johnsbury Road and the other arterials in the planning area.

### **Open Space and Environment**

The undeveloped land at the northwest corner of this planning area contains significant wetlands. Residential development in this area should preserve and enhance these wetlands, preferably through conservation subdivisions (as described in Planning Area 1).

### **Public Facilities**

The Village is currently planning a new sewer system to serve existing high density areas of the Village. Commercial areas and medium density residential neighborhoods, within the current Johnsbury boundaries, fall within the initial sewer service area. Properties along the Fox River and Pistakee Bay are particularly important, to prevent wastewater run-off into these bodies of water.

### **Future Land Use**

#### **Low Density Residential**

This category is applied to existing neighborhoods as well as most of the undeveloped land in this planning area, lying east of Johnsbury Road and south of Miller Road. Due to the large wetland area in the northwest corner of this planning area, conservation subdivision designed (described in Planning Area 1) is recommended.



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## **PLANNING AREA 4**

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Planning Area 4 contains developed medium density residential subdivisions, substantial vacant parcels, and significant wetland and floodplain areas. The planning area contains approximately 540 acres.

### **Boundaries**

North: Johnsburg Road  
East: west boundary of properties fronting on Chapel Hill Road  
South: McHenry border agreement line  
West: Route 31

### **Existing Land Use**

The majority of this planning area is currently agricultural land, much of which is characterized by wetland and floodplain areas. Adjacent to the downtown is the medium density Dutch Creek Woodlands subdivision, including a park bounded by Aspen Drive, Reed Avenue, and Countryside Drive. There are also existing businesses at the intersection of Johnsburg Road and Riverside Drive. The site of the proposed Village waste water treatment plant and potable water production facility is also located within the planning area.

### **Transportation**

This planning area is bounded by the minor arterial Johnsburg Road, and the major arterial Route 31. To alleviate congestion from local traffic on Johnsburg Road, two additional east-west collectors are proposed for the planning area. These new collectors are intended to connect Riverside Drive to Route 31, and should be integrated into new residential subdivisions. Such subdivisions should also be characterized by interconnected road networks but well-spaced entrances onto Johnsburg Road.

Just south of the planning area, a proposed interchange is shown between Route 31 and the Richmond Waukegan Corridor. Although this site is not within the planning area per se, and the Richmond Waukegan Corridor is unlikely to be

constructed within the time horizon of this Comprehensive Plan, the potential of this interchange should be considered. The Village does not desire an interchange of the proposed Richmond Waukegan Corridor at Chapel Hill Road, given that Chapel Hill Road does not have the ability to accommodate a substantial increase in vehicular traffic. An interchange at Route 31 would vastly increase the amount of traffic in the area, reinforcing the need for commercial developments with efficient and safe traffic circulation.

### **Open Space and Environment**

This planning area includes substantial wetland and floodplain areas. If and when residential development proceeds in these areas, new development must be encouraged to maintain and enhance these environmental resources. Conventional development patterns may negatively affect the environmental integrity of the area, including drainage capacity and habitat provision. Cluster or conservation design is recommended for subdivisions in this area (as described in Planning Area 1).

### **Public Facilities**

The Village is currently planning a new sewer system to serve existing high density areas of the Village. Medium density residential neighborhoods in this planning area fall within the initial sewer service area. Properties along the Fox River are particularly important, to prevent wastewater run-off into these bodies of water. In addition, this planning area contains the proposed site of the planned wastewater treatment plant.

### **Future Land Use**

#### *Low Density Residential*

Due to the topography of the land adjacent to Johnsburg Road, and the character of the surroundings, low density residential is applied to the undeveloped areas south of Johnsburg Road and east of Riverside Drive. As detailed in the Open Space and Environment section, this area includes large wetland and floodplain areas

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which should be maintained.

#### Medium Density Residential

This category is applied to the existing subdivisions west of the downtown, as well as to the undeveloped areas between Route 31 and Riverside Drive. Due to topography and safety constraints, new subdivisions should not directly access Johnsborg Road.

The property located between Route 31 and Riverside Drive is characterized by rolling topography and environmental resources. As a result, development should be administered as a Planned Unit Development. The Village may consider high density development, such as townhomes or senior housing, on the west portion of the property. Such development must maintain and enhance the terrain and environmental features of this property.

#### Commercial

The commercial land use designation is applied in the vicinity of the intersection of Johnsborg Road with Route 31. This is a highly visible intersection in the community, on which commercial development will benefit from excellent accessibility.

#### Mixed Use

The mixed use category is applied to the property that straddles Route 31 at the south extent of Johnsborg's jurisdiction. As provided by the annexation agreement governing this property, a wide range of uses are permitted, including residential and non-residential. However, any uses in this area must be compatible with a future freeway interchange at Route 31 and the Richmond Waukegan Corridor, just to the south.

#### Municipal/Public

This designation is applied to the waste water treatment plant.

#### Open Space and Conservation

Private open space is applied to environmentally sensitive areas. These natural resources should be maintained as amenities for Johnsborg residents and for their habitat and drainage characteristics. The existing open space just southwest of Chapel Hill Road is private open space that should be retained for the enjoyment of local residents.

#### **Focus**

Planning efforts for this area should focus on the development of residential neighborhoods which conserve wetland areas, and which contain a highly functional network of collectors to prevent congestion points on arterial roads.

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## **PLANNING AREA 5**

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Planning Area 5 is largely undeveloped at the current time. The planning area contains approximately 1,065 acres.

### **Boundaries**

North: Route 31  
East: Route 31  
South: McHenry border agreement line  
West: McHenry border agreement line

### **Existing Land Use**

This planning area constitutes the southwest extent of the Johnsburg planning jurisdiction. With the exception of some small parcels along McCullom Lake Road, much of the land in this planning area has recently been annexed into the Village and is currently in agricultural use. The McHenry Township Center parkland is also located in this planning area.

### **Transportation**

This planning area contains several important recommended road improvements. The proposal with the greatest impact is the Richmond Waukegan Corridor. This proposed limited access freeway would traverse the southwestern portion of this planning area, and would substantially alter the character and traffic flow of the region. The State of Illinois has already acquired significant portions of the right-of-way necessary to construct this roadway. However, numerous environmental and political concerns affect other portions of the proposed alignment. These concerns could delay construction until long after the time frame of this Comprehensive Plan, or prevent construction altogether. In this light, land use recommendations for this area must be sensible for a location adjacent to a freeway, but uses should not depend upon the freeway to be viable.

The County is also considering a by-pass of Route 31 around McHenry. The Village of Johnsburg, in cooperation with McHenry County, has established a proposed alignment of this by-pass

which will be dedicated to the County upon annexation.

This planning area also includes the recommended extension of several minor arterial and collector roads, to relieve the traffic pressure and congestion on any one road. A system of frontage roads is proposed, particularly if the Richmond Waukegan Corridor is built, to provide a local alternative to using the freeway or arterial roads.

The extension of commuter rail through this planning area is a long-range possibility. A branch of Metra's Union Pacific Northwest Line currently terminates in McHenry, and an extension of this line may occur. To capture the benefits of local commuter rail access in Johnsburg, a conceptual commuter rail station is located in this planning area. However, the 2020 Regional Transportation Plan, generated by the Chicago Area Transportation Study, does not contemplate an extension of this line as of yet. As a result, such an extension is unlikely in the near future.

However, when a commitment is secured to build a commuter rail station, transit-oriented development (TOD) should be considered. TOD is broadly defined as a mixed use community within an average 2,000 foot walking distance of a transit stop and core commercial area. TODs mix residential, retail, office, and public uses, in a walkable environment, making it convenient for residents and employees to travel by transit, bicycle, foot, or car. The Village should encourage transit-oriented development in this area as it grows.

### **Open Space and Environment**

This planning area includes significant wetland and floodplain features. These features should be maintained and enhanced as the Village grows. Particularly if either the McHenry by-pass or the Richmond Waukegan Corridor are built, this planning area could become one of the most heavily traveled and highly developed areas of Johnsburg, and so the provision of adequate open space will be crucial to maintain the quality of life. In addition, the Village should support

McHenry Township in their efforts to maintain the existing park facilities. McHenry Township's Adams Park should be expanded, or, if this is not feasible, a new community park should be added adjacent to Adams Park.

### **Public Facilities**

As part of a regional trail system, the McHenry County Conservation District bike path easement is provided along the railroad line that cuts through this planning area. As provided by annexation agreements governing the parcels in this area, local property owners will dedicate land for local links to this bikeway.

As a condition of the recent annexations in this parcel, the Village has agreed to serve these areas with public sewer and water lines. Individual property owners will be responsible for the construction of necessary sewer and water mains within their own property, and will pay required wastewater treatment fees.

### **Future Land Use**

Large portions of this planning area have recently been annexed into the Village. The annexation agreements allow for largely commercial and industrial use, but do provide the option for a moderate amount of residential use if the non-residential uses are not marketable.

Many of the subject properties have not yet been subdivided. When subdivision is undertaken, the following guidelines should be applied:

1. A detailed set of Covenant, Conditions and Restrictions should be made part of the final approved documents for each subdivided parcel or portion thereof.
2. Each set of CCRs should clearly define the standards for high quality architecture, landscaping, structure siting, parking, lighting, refuse storage and signage.
3. The design of collector streets should be viewed as part of a Village-wide system. Specifically:

- a. Internal road circulation should be provided, so that traffic is contained within the area and not forced out onto arterial roads.
- b. Adequate routes must be provided for emergency service vehicles, including fire and police.

Conceptual land use plans for this planning area have been developed and are currently under review. The future land use designations depicted on the land use plan and listed below reflect the concepts from the most recent plan for the area.

#### Medium Density Residential

This land use category is applied to the properties located east of the railroad and west of the commercial properties fronting Route 31. It is important to note that the floodplain traverses through these properties.

#### High Density Residential

This land use category is applied to the properties located west of the railroad (excluding a set of properties located at the intersection of McCullom Lake Road and the proposed McHenry by-pass).

#### Commercial

This land use category is applied to those properties fronting along Route 31. Commercial uses will benefit from the accessibility and visibility of these sites, and can serve nearby residential neighborhoods as well as a larger market area via Route 31. The properties located at the intersection of McCullom Lake Road and the proposed McHenry by-pass are particularly suitable for commercial use.

In addition, the property located east of the railroad and between the Richmond Waukegan Corridor and McCullom Lake Road is another potential location for commercial use. This particular location adjacent to the conceptual commuter rail station. As such, the commercial uses for this location may be oriented towards a potential TOD development around the

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commuter station.

### Open Space and Conservation

This land use designation is applied to environmentally sensitive areas. These natural resources should be maintained as amenities for Johnsborg residents and for their habitat and drainage characteristics. In particular, the floodplain is the most significant environmental feature in the planning area and is appropriately designated as a private open space.

The township park should be maintained as public open space. Another open space is proposed for the property located east of the railroad and just northwest of the township park.

### **Focus**

Planning efforts for this area should focus on determining the viability and alignment of the major transportation corridors. Major road alignments and time-frames will have a dramatic impact upon the character of development in this planning area. In turn, proposed land use designations for the area will help determine smaller segments of the overall transportation system that fit with the major transportation corridors.

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## **PLANNING AREA 6**

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Approximately one third of Planning Area 6 is developed with low and medium density residential development. There is also a small commercial development, but most of this planning area is vacant agricultural land. The planning area contains approximately 1,030 acres.

### **Boundaries**

North: south boundary of Miller Oaks Estates  
East: Spring Grove Road  
South: Johnsburg Road  
West: Route 31 and proposed Ringwood border agreement line

### **Existing Land Use**

Adjacent to Johnsburg Road, this planning area is developed with residential subdivisions of both low and medium density (including Dutch Creek Estates and Claremont Hill). This planning area also contains a small strip commercial center along Route 31. The remainder of the planning area contains vacant agricultural land.

The western boundary of this planning area is formed by a proposed border agreement with the Village of Ringwood.

### **Transportation**

This planning area is bounded on the southwest by the major arterial, Route 31. Johnsburg Road and Spring Grove Road, minor arterials, form the southern and eastern boundaries, respectively, of the planning area. The extension of S. Solon Road is recommended through this planning area as a minor arterial, to connect with the McHenry by-pass.

A number of improvements are recommended for this planning area, particularly to improve upon the network of collector roads. In the east-west direction, an extension of Lakewood Avenue (except through floodplain areas) is recommended. This road could relieve some of the traffic which might otherwise be forced onto

Johnsburg Road. This road could eventually turn northwards to Harts Road, just west of the planning jurisdiction.

Several new collectors are anticipated in the north-south direction, including extensions of Hamlin Drive, Riverside Drive, Farmington Lane, and Dutch Creek Lane. Where feasible given constraints of topography and natural features, each of these roads should be extended at least as far as Ringwood Road. These extensions are critical to provide alternative routes to the major and minor arterials in the area, and to avoid congestion at the existing single access points to developed subdivisions.

Finally, street extensions are also recommended for local streets such as Buchanan and Fillmore Roads. Similar to the collector extensions, these roads will alleviate congestions on major roads by providing alternate routes.

### **Open Space and Environment**

Like many others in Johnsburg, this planning area contains significant wetland and floodplain areas. Where residential development is contemplated in proximity to these resources, conservation and cluster design is recommended (see Planning Area 1).

### **Public Facilities**

The Village is currently planning a new sewer system to serve existing high density areas of the Village. Medium density residential neighborhoods in this planning area fall within the initial sewer service area.

### **Future Land Use**

#### Low Density Residential

This category is applied to the majority of the land in this planning area, including existing subdivisions at this density. Low density residential development will be compatible with existing subdivisions and the rural character of the Johnsburg area.

#### Medium Density Residential

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This category is applied to the existing Claremont Hill subdivision, which is developed at a medium density. Medium density residential development is also recommended west of Pioneer Road/S. Solon Road and north of the commercial node along Route 31.

#### Commercial

Commercial land use is planned for those parcels along Route 31 northwest of the intersection with Johnsburg Road. There is already a small strip commercial center in this area, and additional community and regional retail uses would be appropriate along this highly visible and accessible corridor. Any development should be consistent with the designation of Route 31 as a Strategic Regional Arterial by the Illinois Department of Transportation, particularly with regard to ingress and egress.

#### Office/Research

This category applies to the area lying west of the Claremont Hill subdivision. This area is not currently annexed into the Village of Johnsburg. Office and research uses can act as a transition between the commercial uses on Route 31 and the residential uses to the northwest and east.

#### Private Open Space

This land use designation is applied to environmentally sensitive areas. These natural resources should be maintained as amenities for Johnsburg residents and for their habitat and drainage characteristics.

#### **Focus**

Planning efforts for this area should focus on the development of non-residential properties along Route 31, in a manner that does not negatively impact the traffic flow of this major arterial while serving nearby residential neighborhoods.

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## **PLANNING AREA 7**

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The majority of Planning Area 7 contains agricultural land. There are also existing low density residential subdivisions, and a property which was recently purchased by the McHenry County Conservation District. The planning area contains approximately 790 acres.

### **Boundaries**

North: Ringwood Road  
East: Spring Grove Road  
South: south boundary of Miller Oaks Estates  
West: proposed Ringwood border agreement line

### **Existing Land Use**

This planning area contains existing low density residential development, including Miller Oaks Estates, Windy Prairie Acres, and Huntington Woods. Most of the remaining land is agricultural, including large areas characterized by floodplain and wetland conditions.

### **Transportation**

This planning area is bounded by two minor arterial roads, Spring Grove Road and Ringwood Road. Pioneer Road, a collector, runs in a northwesterly direction through the west portion of the planning area. The transportation plan recommends that Pioneer Road be realigned, to intersect with the future extension of the minor arterial S. Solon Road.

There are several additional collectors proposed for the area, with the intention of distributing traffic throughout the Johnsburg region. In the east-west direction, a link between Miller Oaks Drive and the southernmost street in Windy Prairie Acres is proposed.

In the north-south direction, new and extended collectors are recommended at Creek Lane, and between Riverside Drive and Windy Prairie Acres. The extension of Riverside Drive passes adjacent to the property recently acquired by the McHenry County Conservation District.

In addition, any new subdivisions should provide interconnections to existing and future development. Existing local streets should be extended into adjacent parcels whenever topography and environmental conditions allow.

### **Open Space and Environment**

This planning area includes a 115 acre parcel which was recently purchased by the McHenry County Conservation District.

### **Future Land Use**

#### *Estate Residential*

This category is applied to the most western portions of this planning area, where subdivisions with lots of up to five acres may be appropriate. New residential development should maintain the rural character of the Johnsburg area, by providing significant buffers along arterial and collector roads. Landscaping and berms are recommended to maintain the rural character of major roads. As mentioned above, interconnections should be provided among subdivisions to allow for an efficient distribution of traffic. Conservation and cluster subdivision design (see Planning Area 1) is strongly encouraged, particularly in the vicinity of conservation areas.

#### *Low Density Residential*

This land use category is applied to the majority of this planning area. Development should be subject to the same guidelines as for Estate Residential above.

#### *Commercial*

Commercial development is anticipated surrounding the intersection of Ringwood Road and Spring Grove Road. This location is intended to be a community level retail center, which may include a grocery store as a major anchor. Site design should be carefully examined to ensure attractive development with interconnections between uses.



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### Open Space and Conservation

This land use designation is appropriate for the property recently acquired by the McHenry County Conservation District, as well as for environmentally sensitive areas. These natural resources should be maintained as amenities for Johnsburg residents and for their habitat and drainage characteristics.

#### **Focus**

Planning efforts for this area should focus on the maintenance and enhancement of natural features. Such features enrich the quality of life for local residents and perpetuate the countryside character of Johnsburg.

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## **PLANNING AREA 8**

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Planning Area 8 consists largely of developed residential neighborhoods within the municipal boundaries, as well as undeveloped agricultural land. This planning area contains approximately 870 acres.

### **Boundaries**

North: Ringwood Road  
East: Johnsborg Road  
South: Johnsborg Road  
West: Spring Grove Road

### **Existing Land Use**

The east and south portions of this planning area consist of residential neighborhoods, most of which have been annexed into the Village of Johnsborg. Generally, these neighborhoods are developed at a medium density. In addition, this planning area also contains the historic Johnsborg Community Center, James C. Bush Elementary School, Johnsborg Junior High, and St. John the Baptist Catholic Church and School. The remainder of the planning area, in the northwestern sections, is undeveloped agricultural land.

### **Transportation**

This planning area is bounded by three major arterial roads: Ringwood Road, Johnsborg Road, and Spring Grove Road. The remainder of the roadways in this area are neighborhood streets. Currently, the planning area lacks continuous north-south and east-west collectors.

The proposed road network includes new collectors in the north-south direction and in the east-west direction. These collectors will provide a local alternative to the major arterials, reserving those roads for longer trips. The proposed collectors are generally extensions of existing roads and should be continued into adjacent planning areas.

Local streets should also provide connectivity between adjacent subdivisions. Arrows show

potential points of connections, where existing road stubs should be continued into neighboring development. Additional access for the school is desirable.

### **Open Space and Environment**

This planning area includes small areas of wetland, floodplain, or poor soil. These features pose constraints to development and should be conserved.

### **Future Land Use**

#### Low Density Residential

This category is applied to mostly undeveloped areas in the planning area. Development in this area should strive to maintain the rural character along Spring Grove Road and Ringwood Road. Recommended measures to achieve this goal include landscaped buffers and undulating berms.

#### Medium Density Residential

This category designates existing developed residential neighborhoods in this planning area.

#### Commercial

Commercial development is anticipated surrounding the intersection of Ringwood Road and Spring Grove Road. This location is intended to be a community level retail center, which may include a grocery store as a major anchor. Site design should be carefully examined to ensure attractive development with interconnections between uses.

#### Municipal/Public

This category is applied to the school sites, which should be maintained and improved in terms of accessibility.

#### Open Space and Conservation

This land use designation is applied to environmentally sensitive areas. These natural resources should be maintained as amenities for Johnsborg residents and for their habitat and

drainage characteristics. In addition, the school yards contain recreational fields for students and local residents, which should be maintained and enhanced.

### **Focus**

Planning efforts for this area should focus on the development of low density residential neighborhoods that maintain the rural vistas along Spring Grove Road and Ringwood Road.

### **Planning Area 9**

The majority of Planning Area 9 is undeveloped, with the exception of a small number of existing commercial and institutional sites. The planning area contains approximately 785 acres.

### **Boundaries**

North: Miller Road  
East: Johnsborg Road  
South: Ringwood Road  
West: Spring Grove Road

### **Existing Land Use**

In the northeast portion of this planning area along Johnsborg Road, are several existing intense commercial uses on lots of at least two acres. There is also a small chapel at the intersection of Johnsborg Road with Ringwood Road, and the high school is located along Ringwood Road midway between Johnsborg Road and Spring Grove Road. Two parcels along Spring Grove Road, just south of the centerline of Section 6 in Township 45, Range 8, are zoned for commercial and medium density residential, and a new residential subdivision has been platted midway between Miller Road and Ringwood Road. The remainder of the planning area is undeveloped.

### **Transportation**

This planning area is bounded on three of four sides by important minor arterials in Johnsborg: Johnsborg Road, Ringwood Road, and Spring Grove Road. Miller Road, a collector, also

connects this planning area with Route 12 to the northeast.

To improve internal connectivity in this planning area, three new collector routes are proposed, although they do not follow section lines due to existing platted subdivisions. In the east-west direction, Broadway Road should be extended from the east to existing east-west roadways. Two north-south collectors between Ringwood Road and Miller Road should also be provided where possible, preferably in alignments that will also connect to collectors south of Ringwood Road.

### **Open Space and Environment**

This planning area contains the community high school, which contains fields and open space for recreational opportunities. The Village can work with the Johnsborg Community School District to ensure that these facilities are well-maintained and provide an asset to the community. Hiller Park is also located in this planning area.

### **Future Land Use**

#### *Low Density Residential*

This category is applied to the majority of the planning area. For the most part, low density residential is appropriate for those properties which would not particularly benefit from the accessibility and visibility afforded by Spring Grove Road and Johnsborg Road. Cluster or conservation design (as described in Planning Area 1) are appropriate for residential development in this planning area, particularly in the vicinity of environmental resources such as wetlands.

#### *Medium Density Residential*

This category is applied to the parcels adjacent to existing medium density residential neighborhoods and non-residential uses. This category also includes the small chapel at the intersection of Johnsborg Road with Ringwood Road, which should be preserved as a historic landmark

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### Commercial

This category is applied to those properties already zoned or developed for commercial use, and to the areas surrounding the intersections of Spring Grove Road with Ringwood Road and Miller Road. Generally, commercial businesses are more appropriate along major arterials such as Spring Grove Road.

### Office/Research

This category is applied to an existing office/light industrial. Roadways should act as a transition between office/research uses and residential developments to the east.

All developments in this area should be subject to the design guidelines described in Planning Area 1 for commercial and office/research uses.

### Light Industrial/Business Park

This category is applied to the properties along Johnsburg Road which are currently zoned or used for warehousing and similar uses.

### Municipal/Public

The high school is a major community facility in this planning area, and serves the entire Village. The Village should support the enhancement of the amenities associated with the high school (such as recreational fields). A connection to the high school should be a high priority in the implementation of the community bike path system.

### Conservation and Open Space

This land use designation is applied to environmentally sensitive areas. These natural resources should be maintained as amenities for Johnsburg residents and for their habitat and drainage characteristics. Hiller Park, north of the high school, is public open space.

### **Focus**

Planning efforts for this area should focus on the creation of an attractive office park, to diversify employment opportunities and the tax base of Johnsburg, while maintaining a high quality image.

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## **PLANNING AREA 10**

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Planning Area 10, which forms the northwest extent of Johnsburg's planning jurisdiction, is largely undeveloped agricultural land. The planning area contains approximately 945 acres.

### **Boundaries**

North: Miller Road (extended west)  
East: Spring Grove Road  
South: Ringwood Road  
West: Pioneer Road

### **Existing Land Use**

Currently, this planning area is completely undeveloped, with the exception of a small number of residences on large parcels.

### **Transportation**

This planning area is bounded on the south and east by major arterials (Ringwood Road and Spring Grove Road, respectively). South Solon Road is recommended as a new north-south minor arterial, to eventually connect to the McHenry by-pass.

There are also several collectors in this planning area to serve future growth. Pioneer Road forms the western boundary. Miller Road forms the northern boundary of the planning area between Spring Grove Road and S. Solon Road. It is recommended that Miller Road be extended to Pioneer Road.

Other recommended collectors for this planning area include an east-west collector along the Broadway Road alignment; and north-south collectors between Miller Road and Ringwood Road. These new north-south collectors should be aligned with collectors in adjacent planning areas, to ensure a continuous network of roadways. Similarly, local streets should also be connected between subdivisions and aligned across arterials.

## **Open Space and Environment**

A floodplain corridor runs through the center of this planning area in a north-south direction. This corridor should be maintained and enhanced.

### **Future Land Use**

#### *Estate Residential*

This category is applied to the majority of the planning area. Development in this area should strive to maintain the rural character along Ringwood Road, Pioneer Road, S. Solon Road, and Miller Road. Recommended measures to achieve this goal include landscaped buffers and rolling berms. Cluster or conservation design (as described in Planning Area 1) are appropriate for residential development in this planning area, particularly in the vicinity of environmental resources such as wetlands.

#### *Low Density Residential*

This category is applied to those areas lying north of Ringwood Road. Development in this area should strive to maintain the rural character along Ringwood Road and S. Solon Road. Recommended measures to achieve this goal include landscaped buffers and rolling berms. Cluster or conservation design (as described in Planning Area 1) are appropriate for residential development in this planning area, particularly in the vicinity of environmental resources such as wetlands.

#### *Office/Research*

This category is applied to the majority of the areas between Spring Grove Road and the proposed new north-south collector to the west. The collector will act as a transition between office/research uses and residential developments further west.

This category is intended to provide a location for high-quality campus-like development in the Village. All developments in this area should be subject to the design guidelines described in Planning Area 1 for commercial and office/research uses.

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### Commercial

Commercial development is anticipated surrounding the intersections of Spring Grove Road with Ringwood and Miller Roads. This location is intended to be a community level retail center, which may include a grocery store as a major anchor. Site design should be carefully examined to ensure attractive development with interconnections between uses.

### Private Open Space

This land use designation is applied to environmentally sensitive areas. These natural resources should be maintained as amenities for Johnsbury residents and for their habitat and drainage characteristics.

### **Focus**

Planning efforts for this area should focus on the creation of the office park, as described in Planning Area 9, and development of residential neighborhoods when such properties become contiguous to existing development.

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## ❖ VILLAGE CENTER PLAN

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Throughout the nation, village centers of virtually all sizes are experiencing a renaissance. The reasons are numerous, including the strong national economy that persisted throughout the 1990s. Underlying all trends, however, is the simple fact that downtowns are unique mixed-use centers of community life that can not be replicated elsewhere. The general public and stakeholders of many types are now well aware of this fact and they are aggressively working together in partnership to achieve revitalization potentials, not only for their own benefit, but for the benefit of future generations.

This spirit led to the selection of the Village Center of Johnsburg as a unique area within the community in need of a long range vision that will strengthen and maintain it as the historic center of the community. This Plan is designed to provide a guide to the important concerns facing the Village Center, and propose strategies for strengthening it as the civic, social, and commercial heart of the community.

The Village Center is and will continue to be the place to which residents refer as part of the Village's identity and source of pride. The varied character and issues to be addressed in the Village Center requires a comprehensive strategy to unite and enhance it. This Plan focuses on infill and rehabilitation issues that must be addressed to preserve, enhance and expand the quality and market for the Village Center. As new commercial growth naturally moves to larger, vacant sites that better fit modern development standards along Route 31, the viability of the older downtown may be threatened. This Plan provides a proactive action plan to ensure the Village Center remains a source of civic pride, unique shopping opportunities, and economic vibrancy.



Historic Downtown Johnsburg

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### VILLAGE CENTER DEVELOPMENT POLICIES

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An essential step in establishing a vision for the future of the Village Center is the identification of overall goals and the formulation of objectives in support of these goals. Goal and objective statements provide the policy framework on which all land use decisions, both now and in the future, should be supported.

The goal statements below are general guidelines that should be used in directing present and future decisions regarding the development of the Village Center. The goal statements are the basis for the establishment of the specific land use recommendations and the preparation of specific design guidelines for the Village Center. They embody the character of development the community desires to create in the Village Center. Objectives are measurable steps that will implement the goals.

## **Community Form and Character**

### Goal:

A positive community identity for the Village of Johnsburg that creates a sense of civic pride and distinguishes the Village from other McHenry County communities.

### Objectives:

- ▶ Ensure a unified approach to the development of Village Center, whereby a mix of uses, public and civic space, site amenities and creative site planning is required in return for zoning flexibility.
- ▶ Maintain the early-20th century character of the Village Center through sensitive infill projects and conversion of existing homes to commercial use.
- ▶ Establish a public space, plaza, or green in the triangle formed by Johnsburg Road, Fairview Avenue, and Chapel Hill Road, to provide a public gathering place and focal point of the community.
- ▶ Capitalize on proximity to local parks, schools, and institutions by establishing direct pedestrian and bicycle links within and between these features.
- ▶ Visually enhance Johnsburg Road and Chapel Hill Road gateways into and out of the Village Center through historically themed landscaping, lighting, and signs.
- ▶ Upgrade, or aid in the relocation or phasing out, of existing intensive industrial uses which are not compatible with residential and commercial activities in the Village Center.
- ▶ Establish standards of excellence in site planning, architecture, and the design of landscaping, lighting, and signs in all commercial and residential projects.

- ▶ Utilize existing and future open spaces for art fairs, music festivals, farmers' markets, or other civic events to encourage community use of the Village Center and strengthen the market for Village Center businesses.

## **Economic Development**

### Goal:

A balanced tax base, supported by a variety of non-residential uses throughout the Village.

### Objectives:

- ▶ Create an economic development program to actively recruit a variety of new businesses to the Village Center.
- ▶ Identify locations for new service and retail developments to serve the needs of a growing community.
- ▶ Ensure an adequate supply of conveniently located public and private parking lots to serve commercial areas and civic events.

## **Transportation**

### Goal:

A well-balanced transportation system that allows for safe and efficient travel throughout the Village for motor vehicles, pedestrians, and bicyclists.

### Objectives:

- ▶ Require pedestrian interconnections between uses to reduce the need for automobile trips and congestion on streets.
- ▶ Provide a system of pedestrian trails, walkways, mid-block connections, and bikeways that encourages safe and easy circulation throughout existing and future developments within and beyond the Village Center.



- ▶ Discourage the realignment of the intersection of Chapel Hill Road and Johnsborg Road, which would unduly increase traffic speed and reduce the pedestrian orientation of the Village Center.
- ▶ Discourage the introduction of an intersection of Chapel Hill Road with F.A..P. 420.
- ▶ Pursue the signalization of the intersections of Johnsborg Road with Fairview Avenue and Spring Grove Road.
- ▶ Locate parking facilities off major commercial streets, with highly visible connections to such parking.

### **Housing and Residential Development**

#### Goal:

A wide range of quality housing within the Village of Johnsborg.

#### Objectives:

- ▶ Develop land use controls to enhance the appearance and quality of higher density residential projects (such as townhouses). Give consideration to well planned units within the Village Center.
- ▶ Encourage well-designed, walkable, developments with open space amenities designed as focal points.
- ▶ Establish standards of excellence for the design, construction and maintenance of all residential structures, to minimize monotonous development.
- ▶ Provide additional housing options for senior citizens, starter households, and "empty nesters" households without children.

### **Environmental/ Natural Resources**

#### Goal:

A balanced relationship between human activities and the environment that minimizes the adverse impacts of development on natural resources.

#### Objectives:

- ▶ Protect local natural resources, such as the Fox River and tributaries, wetlands, woodlands, and ground water from over-development, pollution, and degradation through the establishment of strict development standards.
- ▶ Design all new developments to create a minimum disturbance to natural drainage patterns, natural landscape, vegetation, and the ability of land to absorb rainfall and prevent erosion.

### **Public Facilities and Services**

#### Goal:

Facilities and services which meet the needs of the citizens and business community and promote new economic development.

#### Objectives:

- ▶ Provide sewer service to the Village Center, particularly to protect the Fox River.
- ▶ Bury utility lines wherever possible and encourage the use of attractive poles to mitigate the appearance of above-grade utility lines.
- ▶ Improve and upgrade older public utility and infrastructure facilities through a program of scheduled improvements.

## **FRAMEWORK PLAN**

The Framework Plan is a planning tool used to direct the development of the Village Center. The Framework Plan is a description of the general planning principles that will guide development. Adherence to these principles will ensure a strong Village Center, and provide guidance in the review of development proposals and public improvements through a unified and coordinated approach.

### **Transportation**

#### **Johnsburg Road**

The primary route through the Village Center is Johnsburg Road, which forms the northern edge of the downtown "triangle." This roadway is currently maintained by McHenry County, but the portion east of Chapel Hill Road may someday be transferred to the Village. The Village encourages the maintenance of the 4-way stop at the intersection of Chapel Hill Road with Johnsburg Road, as well as the offset intersection alignment. The current configuration serves a traffic calming purpose by discouraging high speed through traffic, which would compromise the small town character and pedestrian orientation of the Village Center.

#### **Fairview Avenue**

Fairview Avenue is the primary north-south roadway along the east edge of the Village Center triangle. Originally designed as a minor street, this roadway has become an alternative access route carrying much of the traffic between Chapel Hill Road and Johnsburg Road, in order to avoid the offset intersection of those two roadways. In order to maintain the existing intersection at Johnsburg Road and Chapel Hill Road, Fairview Avenue should be upgraded to continue to serve as the alternate route for through traffic on Johnsburg Road. This will require the addition of a stoplight at the Johnsburg Road intersection. The Comprehensive Plan also recommends the future northward extension of Fairview Avenue to

provide for additional connectivity to neighborhoods to the north.

#### **Chapel Hill Road**

Chapel Hill Road has recently been designated as the eastern bypass of McHenry, and traffic loads on this road have increased dramatically as a result. This road forms a major gateway to the Johnsburg Village Center. As described above, through traffic is encouraged to use Fairview Avenue north of the Fox River bridge, rather than Chapel Hill Road, to maintain lower traffic volume and speed on the latter.

#### **St. John's Avenue**

St. John's Avenue is a minor residential road which extends north from the Johnsburg Road/Chapel Hill Road intersection to the historic St. John's Catholic Church. This roadway could become an extension of a pedestrian oriented Village Center. Residences could be converted to non-residential use, forming a "main street" atmosphere with the dramatic focal point of the church at the northern end. Implementation of this part of the Plan should be considered a lower priority, and long term strategy that would serve as a logical extension of the Village Center as demand warrants.

#### **Pedestrian and Bicycle Access**

Ample pedestrian and bicycle facilities will greatly improve the visibility and attractiveness of the Village Center, by providing links within and between the Village Center and the surrounding community features. At a minimum, the maintenance and connectivity of sidewalks should be improved. Bike paths or lanes should also be constructed, particularly along Johnsburg Road between the Village Center and the Township park facilities lying on the west side of Route 31.

Mid-block pedestrian connections are also recommended to provide easy access to public parking lots behind the primary commercial areas in the triangle area. Areas between buildings

should be landscaped with benches and decorative lighting to provide an attractive green space in the Village Center.

### **Redevelopment and Infill Opportunities**

The success of the Village depends on enhancing existing properties while encouraging sensitive development on vacant or other infill sites having redevelopment potential. The recommended opportunity areas, shown on the Framework Plan, are prioritized based on potential availability and on their contribution to the character and environment of the Village Center, and their ability to stimulate private development on adjacent sites. Design and development guidelines for these projects are contained in the next section.

#### Conversion and Infill Opportunities

Throughout the Village Center, the Framework Plan Map identifies areas where conversion and infill activities are appropriate as near term development opportunities. Conversion involves the rehabilitation and reuse of residential homes for commercial use. Such conversion must maintain the residential character of the building and site. Infill activities involve new construction on underutilized or vacant sites. New construction should also respect the historic character of the Village Center, both in terms of architecture and site design.

#### Long Term Redevelopment Opportunities

The Framework Plan Map identifies a number of Long Term Redevelopment Opportunities. These represent important locations for uses that would support and enhance the viability of the Village Center. However, due to the presence of existing businesses with significant investment, these sites are likely to have higher acquisition and relocation costs. As such, redevelopment may not happen in the near term, and should be considered when properties become available or in response to private proposals.

The primary Long Term Redevelopment Opportunities are the "Town Center" in the middle of the triangle area; the Johnsborg State Bank site (where redevelopment by the Bank itself is currently contemplated); the intense industrial uses along Chapel Hill Road (which do not contribute to the desired character or pedestrian orientation of the Village Center and could best be accommodated elsewhere in the Village); the farmstead currently for sale along Johnsborg Road; and the older riverfront hotel on the south side of the Fox River along Chapel Hill Road. General land use recommendations are shown for each redevelopment opportunity on the Framework Plan Map.

### **Gateways**

Several special gateway locations that can be enhanced to define entrances and create a strong identity for the Village Center, include: Johnsborg Road at Church Street and Spring Grove Road, and along the Chapel Hill bridge over the Fox River. A range of entry design and appearance improvements should be undertaken at each gateway location. The following is a general description of the key elements which may be used to define each gateway:

- ▶ Landscaping - distinctive accent plantings should be provided at each gateway location
- ▶ Wayfinding Sign - distinctive identity signs directing people to the Village Center
- ▶ Lighting - special or unique lighting fixtures could be provided to highlight and accent each gateway feature.

### **Streetscape Improvements**

The image of the Village Center as viewed from the car as well as by the pedestrian is defined by the quality of the building architecture and the public space between the street and private property. Potential streetscape improvements to public spaces include street trees, decorative lighting, street furniture (such as benches and trash bins), special paving treatments, burial of

utility lines, etc. In general, a greater intensity of pedestrian amenities should be provided in the "triangle area," while the outlying areas need only receive fewer improvements such as lighting and street trees.

### **Focal Points**

The focal points are special places identified within the Village Center. Focal points combine gateway and streetscape improvements to announce the entrance to the Village Center. The focal points identified in the Framework Plan Map are shown at the three corners of the Village Center triangle.

### **Community Features**

The Community Features identified on the Framework Plan Map are special public or semi-public uses which provide amenities or character to the Village Center. Existing features include the library, schools, and the community park. Recommendations for new features include the town center or "commons" in the triangle area, natural preservation areas and park off Reed Avenue, and expansion of public access to and along the Fox River on either side of the Chapel Hill Road bridge.

## **DESIGN AND DEVELOPMENT GUIDELINES**

Similar to most other communities in the region, the Village Center is no longer the sole center for the Village's retailing and service needs; however, it must be able to compete with other areas in the Village and surrounding communities that also offer such services. This can be most effectively done by creating and conserving a high quality environment, with an inviting image, that has its own unique sense of place.

These guidelines are intended as tools for communicating the design intent for future development and evaluating proposals. The overall goal is to ensure quality development that employs sound planning and design principles.

The purpose of such guidelines is not to dictate a specific development plan for the properties located in the Village Center, but rather to identify elements of building design that should be encouraged. The successful implementation of these guidelines will establish a unique image for the Village Center, as well as a distinct and inviting place to live, work, shop, and gather, which offers a unique appeal not found in other nearby commercial areas.

### **Architectural Character in General**

The character of Village Center commercial, mixed use, and residential areas should possess a strong, unified architectural theme that builds upon the history and traditions of Johnsburg. Specifically, the image of the Village Center should emphasize the residential character for which the community is known. While a specific architectural style is not established, many of the buildings in the Village Center were built between the 1920's and the 1940's, and the style of any new development should respect that tradition.

Particularly in the "triangle area", emphasis should be placed on pedestrian areas, with generous areas for out-door seating, benches and



Historic Johnsburg Storefront



Typical building found in historic villages and towns throughout northern Illinois. New buildings should respect the historic character of the Village.



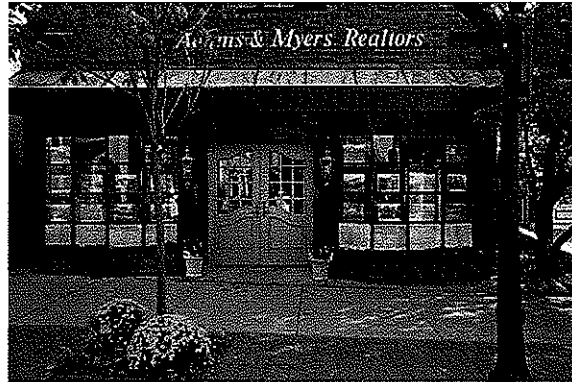
Pedestrian area with paving and seating.

other amenities that will cater to speciality shopping, small restaurants, cafes and service oriented businesses.

Consistent with existing residential neighborhoods in and around the Village Center, residential structures should emphasize a strong and positive relationship of home to street. Housing should be built with front doors oriented towards the street, and garage doors oriented to the rear of the home. Diversity in building scale and appearance is desired to avoid the sameness created by large tract, production type development. Integration of varied housing types, architectural styles, and building materials is recommended.

### Elements of Building Design

- ▶ The Village Center has no one defining style of architecture. Therefore, style is restricted only to the extent that a "residential" character is encouraged. However, each development project should be evaluated based upon the quality of design and its relationship to the residential character of the surrounding area.
- ▶ The choice of materials should relate in character to other properties. They should be of durable quality, suitable for the building type, and architecturally harmonious with adjoining structures. Materials that are similar in texture to those established in the downtown should be used.
- ▶ Colors on a structure should be harmonious with each other and with adjoining structures.
- ▶ Store front windows, covered walkways, and highlighted entrances should be employed to create attractive street corridors.
- ▶ Decorative roof elements and variations in roof height are encouraged to add visual interest and variety to facades. Residential style pitched roofs are encouraged.



Above: An example of design that incorporates recessed entrance, ample windows, good separation between storefront and upper facade, good selection of materials and colors, and good location and size of signs.



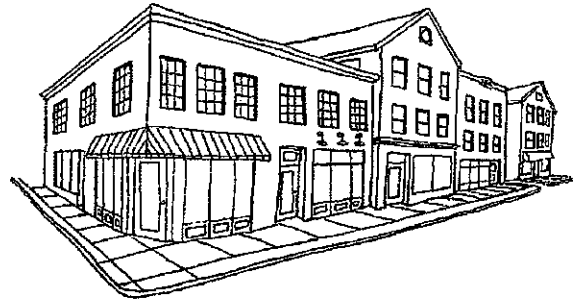
Decorative roof element and change in roof line.

- ▶ Windows, doors, eaves, and parapets should be proportional and relate to one another.
- ▶ Entries should be articulated and should contribute to the volume, mass, proportion, and texture of the building.
- ▶ Monotony should be avoided in design. The roof line at the top of the structure should incorporate offsets and jogs to reduce the monotony of an uninterrupted roof plane.

- ▶ Any equipment, whether on the roof, side of building, or ground, should be screened. The method of screening should be architecturally integrated with the building design in terms of materials, color, shape, and size. Where individual equipment is provided, a continuous screen is desirable.
- ▶ When feasible, all utilities should be located underground.

### **Building Placement and Setbacks**

- ▶ The height and scale of all projects should be compatible with adjoining buildings.
- ▶ All Village Center projects should be oriented towards the primary street on which they are sited.
- ▶ All projects within the Village Center should be planned for safe and comfortable pedestrian movement, and should provide adequate planting and parking, accomplishing a desirable transition with the streetscape.
- ▶ Maximum building setbacks, or "build-to" lines, should be established along Johnsborg Road, Chapel Hill Road, and Fairview Avenue in order to provide continuity and enhance the image of these roadways.



Buildings with a variety of roof lines and types, yet the general height and bulk remain the same.

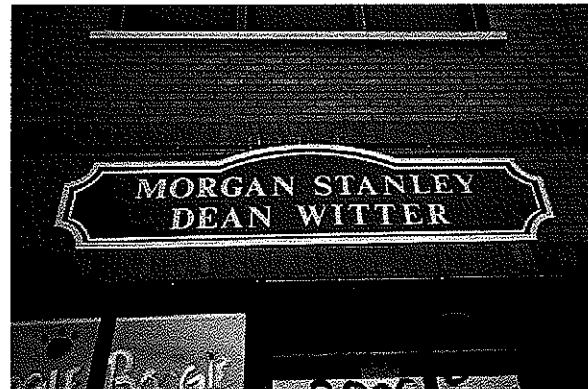


Buildings placed near street with angled parking. The pedestrian environment has been enhanced with trees, brick paving, and a sidewalk width that is adequate for the foot traffic.

- ▶ When located on corner lots in the Village Center, developments should build to the corner property lines to reinforce the integrity of the corner. However, at the primary corner of Fairview Avenue and Chapel Hill Road, a small public plaza or open space may be appropriate.
- ▶ Open space areas should be clustered into larger, landscaped areas rather than equally distributing them into areas of low impact (such as at building peripheries, behind a structure or in areas of little impact to the public view) that are not required as a land use buffer or as a required yard setback.

### Signs

- ▶ All signs should be highly compatible with the building and site design relative to color, material, and placement and should comply with the Village's sign regulations. The Village may consider adopting special sign regulations for the Village Center. Provisions for sign placement, sign scale in relationship with the building, and sign readability should be considered in developing signs.
- ▶ Signs should be placed to avoid disrupting the rhythm of windows and trim or obscuring any of a building's more significant architectural details or ornamentation. Wall signs and projecting signs shall be placed at or above the transom level, below the second story window sill on a multi-story building and below the roof on single story buildings. Roof signs are prohibited.
- ▶ Signs should be sized in proportion to the building and other signs in the district.
- ▶ Where multiple storefronts exist on a single building, the individual signs for each store should relate well to each in terms of height, proportion, color and background value.



Above: Two examples of signs that use compatible materials and colors, and are proportioned to the buildings they adorn.



- ▶ Given the traditional character of the architecture in the Village Center, information and decoration on signs should be limited in order to harmonize with the setting. Colors on signs should coordinate with overall background colors. Signs with a dark background and light colored lettering have the most impact. Lettering should consist of simple, easy to read typefaces, with no more than two (2) letter type families per sign. Generally, the area of the sign covered by the letter forms should occupy a maximum of seventy-five (75) percent of the sign area.
- ▶ Internal illumination and flashing signs are prohibited. Where provided, lighting should be simple in design and/or concealed. Concealed up-lit light fixtures, fixtures of simple design, or fixtures appropriate to the period of the building are recommended.
- ▶ Materials should be compatible with the structure to which the signs are attached and with other signs in the Village Center.
- ▶ Brackets for projecting signs shall be compatible with the period of the structure to which the sign is to be attached.
- ▶ Awnings and canopies should be integrated with the building facade. Awnings and canopies throughout a block should produce a consistent pattern through their size and shape. Graphics should appear only on the overhanging valence, and such valence should be no more than twelve (12") inches in height.

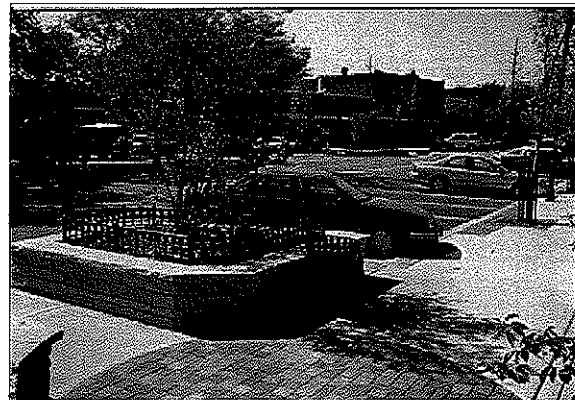
## Parking and Service Areas

- ▶ On-street parking should be provided on Chapel Hill Road, Fairview Road, and Johnsburg Road east of Spring Grove Road. On-street parking preserves the pedestrian scale and density of uses, while providing convenient access to stores and service
- ▶ Off-street parking lots should be located at the rear of commercial uses and should be screened from the public view by either the use of landscaping, berms, wall extensions, or a combination thereof. Parking lots should be limited along Chapel Hill Road, Fairview Road, and Johnsburg Road, as they create a discontinuous commercial frontage (thereby weakening the retail experience).
- ▶ Where parking lots are located to the rear of commercial uses, direct pedestrian walkways and mid-block connections should be provided to allow for easy access to parking facilities.
- ▶ Shared parking lots are encouraged, in which multiple uses can be given credit for the same parking spaces. Such a system works in cases with complementary uses, or uses with differing peak hours (such as churches and restaurants). Village ownership is preferred, although a permanent public access easement may be appropriate in some cases. The Village may reduce parking requirements in the Village Center, and/or may set up a parking fund to which property owners can contribute in lieu of creating on-site parking.
- ▶ Common driveways which provide vehicular access to more than one site are encouraged.
- ▶ Service and loading areas should be located behind or to the side of commercial buildings, hidden from public view. Such areas should be sited so that trucks do not obstruct traffic on public roads while loading and unloading.



Parking areas should be adequately landscaped.

- ▶ When allowed, exterior storage should be confined to portions of the site least visible to public view. Where screening is required, a combination of elements should be used, including solid masonry walls, berms, and landscaping.



Raised masonry planter wall enhances screening and provides pedestrian seating

## Stormwater Detention

- ▶ Small, individual stormwater detention basins should be discouraged. Usually the responsibility of property owners or homeowner associations, smaller basins are more expensive to construct and difficult to maintain. When practical and where feasible, stormwater should be collected in an area-wide detention facility serving the entire Village Center.

## Conversion of Existing Residences

These guidelines specifically address the conversion of existing single family homes in the Village Center into office, service, or small boutique retail uses.

- ▶ The character and appearance of the existing residential structure should be maintained to the greatest extent possible.
- ▶ New construction, where necessary, should be designed with residential character and mass and bulk similar to surrounding residential buildings.
- ▶ Parking should be provided in side and rear yards and should be screened from adjacent uses. Lighting of parking areas should be limited to low bollard type fixtures. Freestanding light poles and flood lights attached to buildings may cause glare onto adjoining residential properties, and should be prohibited.
- ▶ Freestanding signs should be limited to low, monument type signs, not exceeding 4' in height. Internal lighting shall be prohibited. Signs attached to buildings shall be secondary, and used for identification purposes only.



Converted residential structures must maintain the residential character of the building and preserve or enhance the quality of the street scape.

## New Residential Projects

- ▶ Residential designs should respect the established street, lot, and building patterns of the surrounding neighborhood. Specific site conditions, including trees, streams, wetlands, and other significant natural feature should also be considered.
- ▶ Residential character should be maintained in height, bulk, and scale. Techniques such as facade and roof modulation, landscaping, or setbacks should be employed to achieve a reduction in the visual mass of the buildings.
- ▶ Projects adjacent to single-family homes should be sited and designed to provide a transition to the smaller scale of the neighborhood.
- ▶ Residential developments should be oriented to the street, and primary building entrances should be visible from the street. Street-facing entries should be provided to the individual units whenever possible, to strengthen the residential streetscape and create a sense of neighborhood among residents. Street-facing facades should be designed with entries, windows, and/or detailing.
- ▶ Setbacks can be used to create a sense of entry and amenities for ground-floor residents. To reinforce neighborhood character, entrances should be highly visible from the public street, providing a visual transition.
- ▶ Parking lots should be located to the side or rear of buildings to minimize their visual impact on the site. Garages and carports should be located along non-street facing elevations.



New residences should be constructed of high-quality materials. Here, brick and stone are used. Custom homes are preferred to mass-produced tract housing.

- ▶ Parking lots visible from public rights-of-way and within 20 feet of single-family residential use should be screened from view, using shrubs, trees, low walls, berms, or a combination thereof.
- ▶ Vehicle circulation should not conflict with pedestrian circulation or with children's play areas.
- ▶ The site plan should include usable open spaces that are easily accessible by residents and visually removed from parking lots.
- ▶ Higher density residential developments such as townhomes should provide pedestrian connections with the community. On-site pedestrian circulation should also provide convenient links between residential units and common open space, recreation, and parking.



Traditional orientation to the street and a park that creates a strong pedestrian environment.

## **IMPLEMENTATION**

Implementation of the Village Center Plan consists of a wide range of activities, some of which are pro-active while others are reactive. Both, however, are vital to implementing the Plan. Pro-active activities are those in which the Village initiates actions through a proposal, plan, improvement, or regulatory change. Reactive activities are those in which other parties approach the Village with a proposal on which the Village must act.

The initiatives described in this section represent only the pro-active measures, because these are the ones that can be planned for and controlled by the Village.

### **Update the Zoning and Subdivision Ordinances and Zoning Map**

The benefit of this initiative is to put the policies of this Plan into ordinance and map form, and begin to implement the private sector aspects of these policies through ordinance enforcement. For example, the Village may wish to evaluate parking requirements as applied in the Village Center area. Onerous parking requirements that do not recognize shared parking or municipal parking facilities will hamper development in the Village Center. The Village should also evaluate the Planned Unit Development standards to ensure they will result in unified, unique developments in the Village Center. The new B-5 zoning district should also be periodically re-evaluated to ensure that the goals of the district are implemented through its use.

### **Design Guideline Manual**

The Village should require a detailed design guideline manual as a part of the negotiations with developers on redevelopment plans. The manual should be based upon the recommendations contained herein regarding the preferred land uses and design guidelines for the Village Center. The purpose of the design manual is to establish a more detailed set of design standards for such items as building facades and materials,

streetscape (lighting, paving, benches, signs, banners, trees and planting beds), and other public use areas. The design manual will be used with redevelopment plans to ensure that each development in the Village Center conforms to the quality and character desired by the Village.

### **Redevelopment Assistance and Financing**

To encourage redevelopment of priority sites in the Village Center, the Village may need to consider direct municipal involvement and financial assistance.

- Tax Increment Financing (TIF) is a financial tool used by many communities in Illinois and other states to facilitate redevelopment of older deteriorated areas. TIF can be used to make the area attractive for development and redevelopment by eliminating the conditions which inhibit private investment through the use of incremental property tax revenues generated by new development. The municipal cost of such public improvements can be repaid with the increased revenues generated by private investment in real estate within a designated project area.

The key to TIF financing is that funding does not rely on the taxing power of the municipality alone. The property taxing power of all taxing districts with jurisdiction over the land can be employed to provide the necessary financial assistance. This taxing power is only applied to the increase in equalized assessed valuation within the designated project area. In accordance with this Village Center Plan, public improvements may be constructed and financial incentives provided by the Village to encourage the type of private investment that will allow the Village to achieve its goals for this area.

- ▶ Special Service Area (SSA) financing is a taxing mechanism that can be used to finance special or additional services or improvements or facilities desired in the Village Center. It involves levying a tax only on the area (SSA) receiving those services or improvements, rather than burdening the entire community with a debt or operating tax.
- ▶ Property tax abatement is a financing tool that can be used to help attract developers and businesses to the Village Center. Any municipality is allowed to forgive all or a portion of the property taxes owed to it by a firm for a period of time up to ten years.

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## ❖ DESIGN & DEVELOPMENT GUIDELINES FOR THE ROUTE 31 CORRIDOR ❖

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The Design and Development Guidelines presented are intended to establish standards whereby new development will be evaluated to ensure that the high quality character of the Village is maintained and enhanced. The application of the guidelines will also contribute to the long-term vitality and economic health of the Route 31 Corridor by enhancing its physical appearance. By conserving and creating a high quality environment with an inviting image, the corridor will experience renewed vitality and enhanced economic performance.

The Design and Development Guidelines described herein are intended as tools for communicating the design intent for future development, rehabilitation, and redevelopment efforts, and should be used in evaluating proposals. The goal is to build an attractive, recognizable development district with an appealing atmosphere that reflects harmony and continuity in building and site design and streetscape improvements. The objective is to create a unified and distinct development that is pedestrian-friendly, fosters civic pride and ownership, promotes a sense of place specific to Johnsburg, and offers a feeling of security.

The purpose of such guidelines is not to dictate a specific development plan for all the properties located along the Corridor, but rather establish a set of guiding principles and identify elements of building design that should be encouraged.

### **Applications**

As presented, the design and development guidelines apply to all private, non-residential properties located in the Route 31 Corridor. Such guidelines are intended to supplement the development standards of the applicable zoning district and are not intended to be all-inclusive. Other techniques or standards adopted by the Village may be used to satisfy the intent of the requirements.

### **Design and Development Review Process**

Design and development review is a process intended to ensure that both new development and redevelopment efforts enhance the visual quality and identity of the Route 31 Corridor. It establishes architectural and landscape principles and design guidelines that respect the existing development trends, avoid adherence to a rigid style, and promote sensitive rehabilitation of older buildings.

Although design and development review is related to building and zoning codes, each act in different capacities during the development or redevelopment of properties. Building and zoning codes regulate the use of property and set standards for building height, setback, and parking. Design and development review, however, works to ensure that new construction, renovation, and redevelopment efforts are compatible with the character of Johnsburg.

Through development review, citizens can work together with builders, developers, and property owners to protect identified community values through attention to simple design principles.

### **Benefits**

Through the use of design and development guidelines, the Village, citizens, business owners, and users of the corridor all stand to benefit. As applied to the Route 31 Corridor, identified benefits include the following:

- ▶ Protecting and strengthening investment in the corridor;
- ▶ Attracting business and pedestrians to the area;
- ▶ Creating a positive community image;
- ▶ Creating visually appealing and functional buildings;

- ▶ Facilitating quality redevelopment of old assets;
- ▶ Enhancing the community and its potential for the future;
- ▶ Acting as a catalyst for making other things happen.

### Design and Development Guidelines

The guidelines that follow are elements of site and building design that should be encouraged or discouraged to reflect the physical qualities of the Village.

The *Village of Johnsburg Route 31 Corridor Design and Development Guidelines* are arranged to address the following:

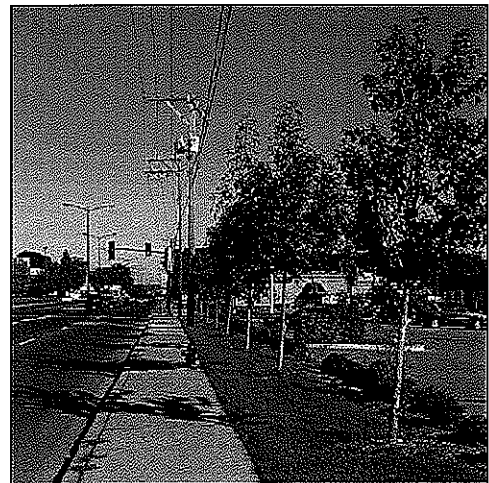
- ▶ Site Design and Building Orientation
- ▶ Architectural Design
- ▶ Building Materials and Color
- ▶ Landscape Design and Screening
- ▶ Pedestrian, Bicycle, and Vehicular Circulation and Access
- ▶ Parking Configuration
- ▶ Signs
- ▶ Building and Site Lighting

These guidelines are intended as a reference to assist the property owner, designer, and/or developer in understanding the Village's goals and objectives for high quality development throughout the Route 31 Corridor. Furthermore, such guidelines compliment existing Codes, Ordinances, and regulations already established and adopted by the Village of Johnsburg.

### Site Design and Building Orientation

The following site planning guidelines primarily address the organization of a project's components in two dimensions. They deal with the location of buildings and site features such as parking lots, open space, and service areas. Thoughtful site planning can minimize a project's impact on its neighbors, increase the quality of the streetscape, and enhance a user's experience of the site.

- ▶ All free-standing singular use buildings should be generally oriented towards the front setback line within a well-landscaped green area, with loading areas, and parking areas located to the side or rear of the site.



- ▶ Free-standing singular commercial and service-oriented structures should be oriented with their major entry toward the street where access is provided, as well as having their major facade parallel to the street. Restaurant uses should orient their major entry toward the principle parking facility with clear paths leading to such entry.
- ▶ Front facades of buildings should be oriented towards Route 31, taking advantage of the exposure created along this road.





- ▶ The development design, including the arrangement and placement of building and site amenities, should create “human scale” and be oriented to the pedestrian.

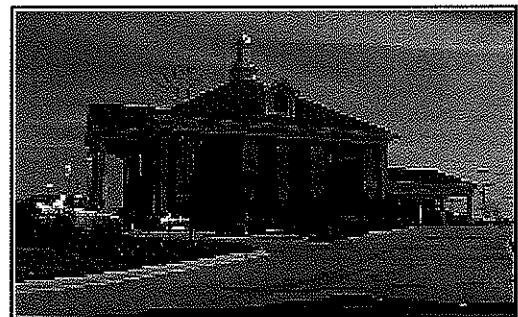


- ▶ Structures and on-site circulation systems should be coordinated to minimize pedestrian/vehicle conflicts and provide cross-access to adjacent properties.
- ▶ Open space areas should be clustered into larger, landscaped areas rather than equally distributing them into areas of low impact such as at building peripheries, behind a structure, or areas of little impact to the public view that are not required as a land use buffer or as a required yard setback.

## Architectural Design

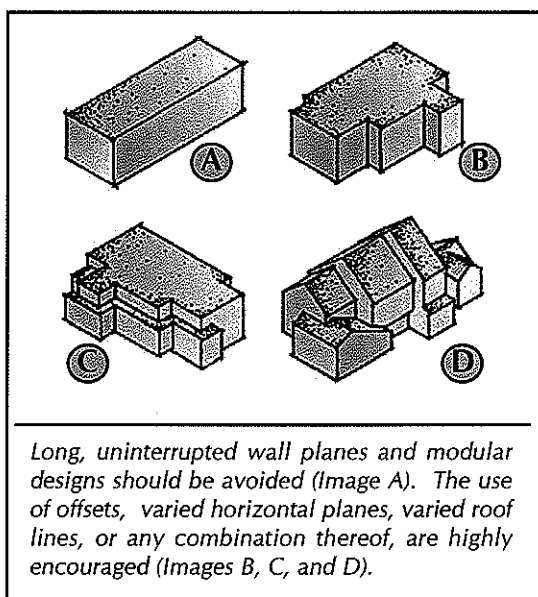
Common architectural guidelines and design elements applied throughout the Route 31 Corridor will help to establish an attractive, unified visual image for the corridor. While the following guidelines apply to all proposed new development, renovations, and redevelopment efforts, architectural innovation is encouraged within this framework.

- ▶ The Route 31 Corridor has no one defining style of architecture, and therefore, style should not be restricted. Each development project should be evaluated based upon the quality of design and its relationship to the surrounding area.
- ▶ Designs should provide visual interest and variety in keeping with the “small town” and historic character of the Village. Buildings should be designed in an attractive and interesting manner to not only define the image of the Corridor, but to reflect that of the Village as well.



- ▶ All sides of buildings should be equally attractive. Architectural details such as texture, pattern, color, and building form used on the front façade should be incorporated on all visible building facades. However, such requirements need not apply to any façade(s) facing areas not generally visible to the public.

- ▶ Building massing should be varied to create a logical hierarchy of building forms; to break up long expanses of façade; to create shade and shadow; and to create a "human scale."



- ▶ Human scale should be created by building massing form, as well as the use of architectural elements such as colonnades, canopies, walkways, street-level display windows, lighting, and a variety of building materials. The incorporation of site design features around the building exterior should further reinforce human scale.
- ▶ Long, uninterrupted wall or roof planes should be avoided and signs, lighting, utilities and services should be integrated with the building design.
- ▶ Monotony should be avoided in design. The roof line at the top of the structure should incorporate offsets and jogs to reduce the monotony of an uninterrupted roof plane.
- ▶ Commercial building facades should provide appropriate proportions of transparent glass in the entry street facing wall to promote visibility between the street and building interiors.

- ▶ Where applicable, window patterns should be compatible with those in adjoining buildings. Windows, doors, eaves, and parapets should be proportional and relate to one another.
- ▶ Each building or storefront should have a clearly defined, highly visible entrance.



- ▶ The scale of buildings should be carefully related to adjacent, or planned pedestrian areas and other structures.



- ▶ Large, dominating structures should be broken up by creating horizontal emphasis through the use of trim; adding awnings, eaves, windows, or architectural ornamentation; use of complementary colors; and landscape materials.

## Building Materials and Color

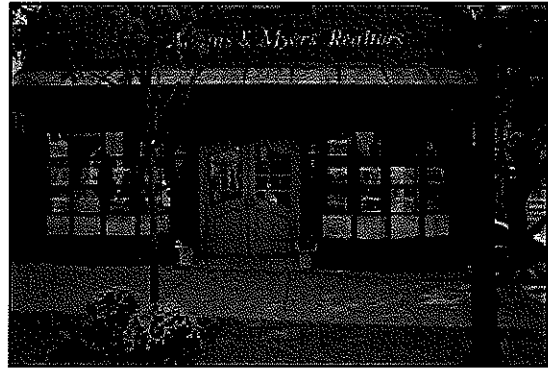
Building materials and color are important elements that often dictate the aesthetic and physical quality of a development. In order to assure the overall quality of development desired for the Route 31 Corridor, the following guidelines should be followed.

- ▶ Predominant exterior materials must be of a high quality. These include brick, wood, limestone, other native stone, and tinted/textured concrete masonry units. Smooth-faced concrete block, tilt-up concrete panels, or pre-fabricated steel panels are prohibited as the predominant exterior building material.



- ▶ Dissimilar materials may be permitted when incorporating other characteristics such as scale, form, architectural detailing and color to make the building compatible with the area.
- ▶ Life cycle and low maintenance requirements should be considered in material selection. For instance, materials with integral color are generally recommended over materials that require painting.
- ▶ Colors for buildings should be complementary colors that work to define the corridor.
- ▶ Façade colors should be of low reflectance, subtle, neutral, or earth tone colors. The use of high intensity, metallic, black, or fluorescent colors is discouraged.

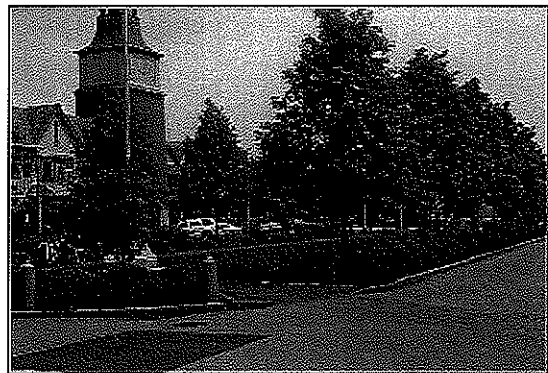
- ▶ Building trim and other details/accents may feature brighter colors, however, neon tubing is discouraged as an accent material.



## Landscape Design and Screening

Landscaping can be a visible indicator of quality development, and is particularly important to passing motorists. Landscaping should be used as an opportunity to visually tie an entire development together by screening parking or service areas, accenting entryways, enhancing the appearance of buildings, buffering automobile traffic, creating an attractive, shaded environment along street edges, and defining circulation for vehicles and pedestrians.

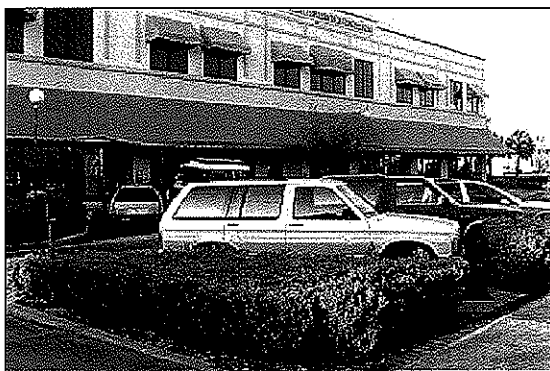
- ▶ Landscaping should define entrances to buildings and parking lots, define the edges of various land uses, provide transitions between neighboring properties (buffering), and provide screening for loading and equipment areas.



- ▶ Landscaping should be in scale with adjacent structures and be of appropriate size at maturity to accomplish its intended purpose.
- ▶ Landscaping should be protected from vehicular and pedestrian encroachment by raised planting surfaces, depressed walks, or the use of curbs.



- ▶ Landscaping should not obstruct visibility at drive and/or parking aisle intersections.
- ▶ Parking lots should be planted to minimize their presence and enhance their appearance. Landscaping for the parking areas should be located in a manner that provides visual relief, shading of the lot, green areas, and screening, while ensuring that the lines-of-site are maintained, both at the time of planting and at plant maturity. Consolidated, large landscaped areas are preferred to break down the visual negative impact of large asphalt areas.



- ▶ Parking lot screening from public rights-of-way and pedestrian walkways is required when existing site characteristics do not adequately screen parking areas. Parking areas in the rear and side of lots require less screening and are therefore encouraged.
- ▶ Landscape islands within parking lots are required to improve not only the appearance of the site, but also the overall on-site vehicular circulation patterns.



- ▶ At a minimum, two (2) shade trees are required per landscaped island and one (1) landscaped island is required for every 15 parking spaces.
- ▶ Trash enclosures, utility boxes, meters, and loading/service areas must be screened from adjacent properties, public rights-of-way, parking areas and pedestrian walkways.
  - Screening for trash enclosures should consist of a solid wall of the same material as the principle building.
  - Roof equipment screening should consist of a parapet wall.
  - All other utility equipment and service areas should be screened with landscape material and/or fencing, equal in height or taller than the equipment being screened.

- ▶ Trash enclosures must be located towards the rear of the site, unless it can be determined that a location within a side yard is more appropriate to the functioning of the establishment. Under no circumstances is it permitted in a front yard.
- ▶ No outside vending machines are permitted for any use along the Route 31 Corridor.

### **Pedestrian, Bicycle, and Vehicular Circulation and Access**

These guidelines are intended to provide improved circulation and reduced vehicular traffic conflict by ensuring that circulation and access patterns create an integrated transportation network for pedestrians, bicycles, and vehicles.

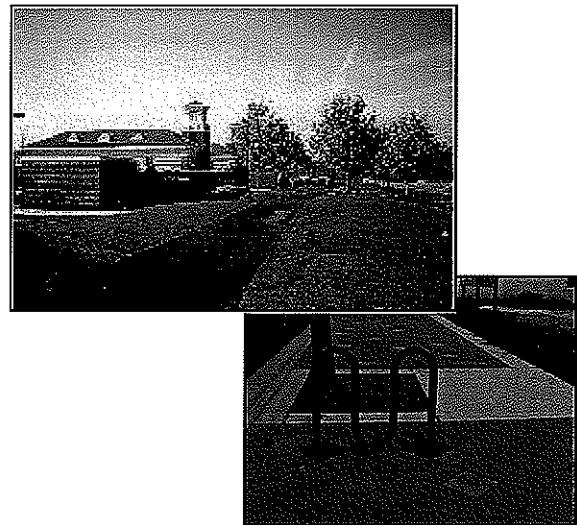
- ▶ The frequency of driveways and other access points should be minimized to avoid conflicts with other traffic patterns.
- ▶ Vehicular connections should be provided from a development site to adjoining streets, driveways, or other circulation systems on adjoining sites.
- ▶ An internal pedestrian walkway should be provided from the perimeter public sidewalk to the principle customer entrance. This walkway must be distinguished from the driving surfaces through the use of special pavers, bricks, or scored concrete to enhance pedestrian safety and the attractiveness of the walkways.



- ▶ Clearly delineated circulation paths from parking areas to building entries should be provided.
- ▶ Sidewalks must be provided along the length of any façade abutting designated parking areas.

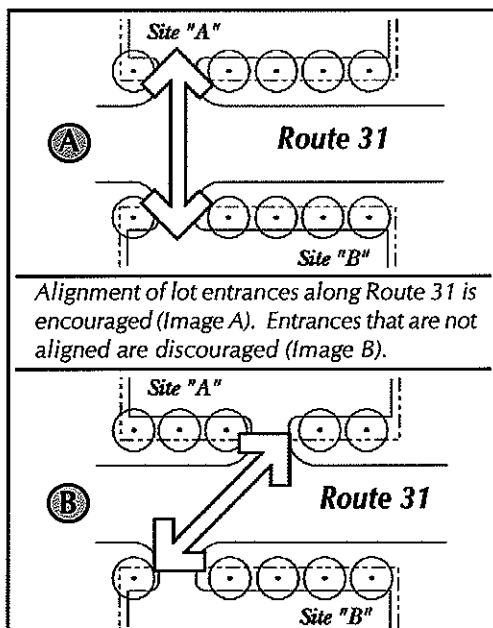


- ▶ Opportunities for bicycle/pedestrian mobility should be enhanced through site design strategies and bicycle/pedestrian access ways that seek to shorten walking distances and increase accessibility between uses.



- ▶ The provision of bike racks or bike parking should be incorporated into all site designs, unless it can be determined that such needs are adequately addressed on adjacent sites or along the public right-of-way.

- ▶ Where appropriate, bike paths and bike connections should be provided or link with other existing or proposed paths that connect to the Prairie Path Trail.
- ▶ For all new developments, the number and location of entrances should be consistent with the existing or anticipated design of Route 31 Corridor and adjacent streets. The specific locations of entrances are largely dependent upon the following factors:
  - The location of existing or planned median breaks;
  - Separation requirements between the entrance and major intersections;
  - Separation requirements between other entrances;
  - The need to provide shared access with other sites;
  - The need to align with previously approved or constructed access points on the opposite side of the street;



- The minimum number of entrances needed to move traffic onto and off the site safely and efficiently.
- ▶ Entrance drives must be designed to provide efficient ingress and egress to the site free from parking spaces backing into the drive.
- ▶ Common driveways that provide vehicular access to more than one site are encouraged.
- ▶ Whenever practical, shared parking between adjacent businesses and/or developments is encouraged.

### Parking Configuration

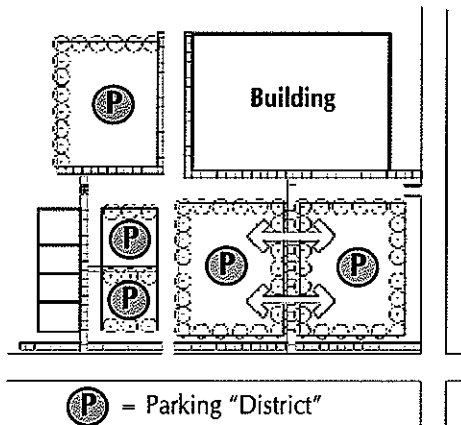
Quality development corridors provide a variety of convenient parking choices consistent with the scale of the development, the location, and the types of stores. A quality place allows exible parking arrangements such as on-street parking and shared parking to minimize an over supply of parking. Large blocks/strips of uninterrupted parking detract from the appearance of a development corridor and create a confusing and sometimes hazardous environment for both motorists and pedestrians.

- ▶ Parking should be discouraged in the front yard setback for lots fronting directly on Route 31.
- ▶ If parking is to occur in the front yard of properties fronting Route 31, increased landscaping and buffering is required to screen parking fields.

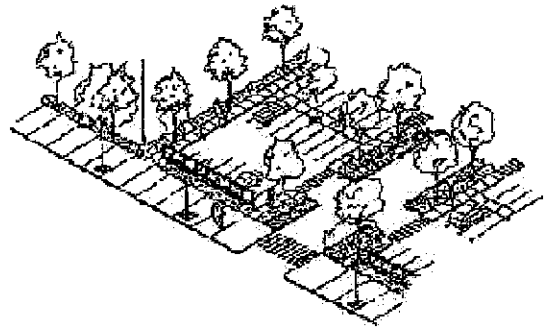




- ▶ Parking aisles should be separated from vehicle circulation routes whenever possible.
- ▶ Where parking areas are connected, direction of travel and parking bays should be similar to reduce conflict at points of connection.
- ▶ Wherever appropriate, parking access points, whether located on front or side streets must be located as far as possible from street intersections so that adequate stacking room is provided. The number of access points should be limited to the minimum amount necessary to provide adequate circulation.
- ▶ Parking areas designed to accommodate a significant number of vehicles should be divided into a series of connected smaller lots or "districts". This is principally applicable to strip developments or multiple tenants developments that share a common parking area.



- ▶ First aisle parking stalls should be set back a sufficient distance from the curb to avoid traffic obstruction. Drive aisle throats should be sufficient depth to avoid vehicle stacking into the street.
- ▶ Parking areas must be landscaped, receiving interior as well as perimeter treatments.



- ▶ Parking areas should be separated from structures by either a raised concrete walkway or landscaped strip, preferably both. Arrangements where parking spaces directly abut structures should be avoided.
- ▶ Utilization of an opaque wall, landscaping, or combination of walls, berms, and landscaping material to screen any parking at the entry periphery is required. Changing the grade of the parking lot from existing street elevations may aid in obscuring views of automobiles while promoting views of architectural elements of the on-site structure(s).

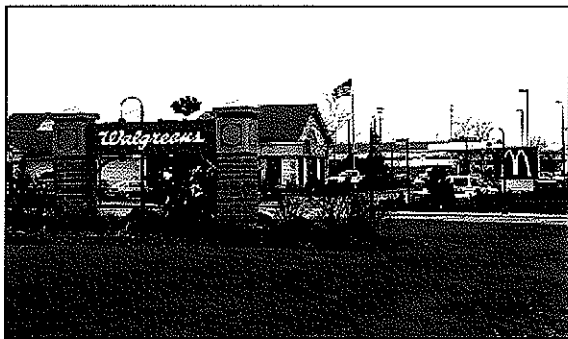


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## Commercial Signs

Commercial signs is a key element to the overall image and character of the Route 31 Corridor. The types of signage that are often associated with large commercial transportation corridors include tall, pole-mounted signs, pylon signs, wall-mounted signs, site-specific monument, and directional signage of all shapes and sizes. The shape, size, and scale of signs are indeed some of the most important features because they have the most visual impact on the appearance of a corridor. Therefore, types of materials and site-specific features, such as monuments, have a significant influence on building a high-quality character for a corridor. The guidelines below outline measures that will help prevent visual clutter and foster a strong community image.

- ▶ Provisions for sign placement, sign scale in relationship with the building, and sign readability should be considered in developing the signing concept.
- ▶ Signs should be of scale and proportion in design and form a visual relationship to the building and surroundings. They should not be applied to the wall or windows of the building in such a way as to interfere with architectural details nor disrupt the rhythm of the columns and fenestration. They should be of a size, location and design that does not obscure a building's important architectural details or overwhelm the architectural elements of the facade or building.



- ▶ All signage should be highly compatible and consistent with the building and site design relative to color, material, placement, and forms used, and should comply with the Village's sign regulations.
- ▶ Signs typically vary in size, vertical location, typeface, and color scheme. Each sign along the Route 31 Corridor should be compatible with signs on adjoining premises and should not compete for attention.
- ▶ The number of wall mounted signs per storefront should be kept to a minimum of one (1) per business, except for corner lots where it can be determined that it is appropriate to have one (1) sign per front facing a road.
- ▶ Freestanding signs should be limited to ground or monument signs, and should be designed with consistent design elements, such as a base material, height, and lettering style, to create a visual continuity and quality to development.



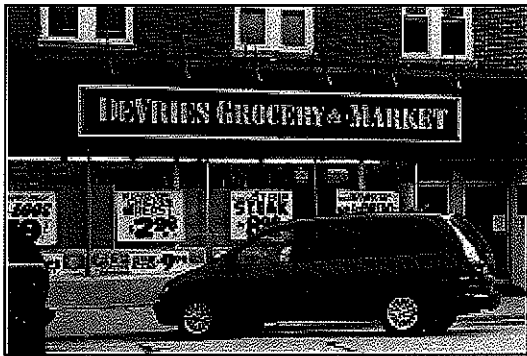
- ▶ Pylon or pole signs within the corridor should be discouraged. If pole signs are required, they should not exceed a height of 15'.
- ▶ All signs should be designed as an architectural element of the structure. The color, materials, and lighting should be restrained and harmonious with the building.



- ▶ All signs should be made of high quality, durable material. All freestanding monument signs must have a base and frame of masonry material complementary to the materials on the primary building with which the sign is associated.

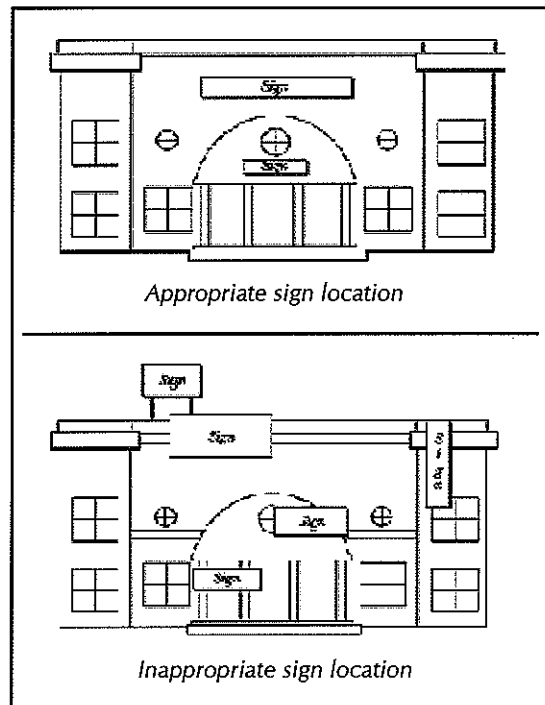
- ▶ A limited number of colors should be used. Background colors that match the building color (or neutral), and light graphics on a dark background are encouraged.

- ▶ As applied to wall-mounted signs, preferred approaches for the corridor include the use of flood lights attached on gooseneck fixtures from the top of the roof or top of the sign, wall-wash lights mounted behind opaque sign letters or elements, and up-lights mounted in an open area beneath the sign.



- ▶ The use of neon lighting and lettering on primary signage should be limited. Its use on secondary signage or as an accent may be appropriate, however, high-intensity sign lights or the excessive external illumination of any sign is inappropriate.
- ▶ Flashing signs and signs that incorporate any type of movement, either in design or display, should be prohibited.
- ▶ Horizontal or flat wall signs are appropriate for structures located along the corridor. Such signs should be mounted above the window and below the roof, or above the window

below the floor line of the next story (a minimum of 8'-0" and a maximum of 15'-0" above grade). Signs should not protrude more than 9 inches from the face of the building.



- ▶ Signs perpendicular to the building wall are not appropriate.
- ▶ Changeable letters are prohibited on primary freestanding signs and should not occupy more than the 30% of wall signs.
- ▶ Signs (wall signs, three dimensional words or letters) should provide information simply and legibly. The message should be clearly conveyed; complex type faces should be avoided. Graphic elements should be held to the minimum needed to convey the sign's major message.
- ▶ Signs (wall signs, three dimensional words or letters) should provide information simply and legibly. The message should be clearly conveyed; complex type faces should be avoided. Graphic elements should be held to the

- ▶ Signs of any nature are not permitted to project from roofs.
- ▶ Marquees are not permitted as part of any sign, except for theaters.

### **Building and Site Lighting**

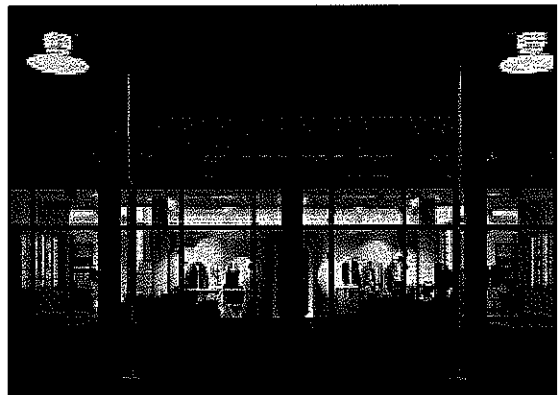
Lighting has a significant influence on the appearance, sense of safety, and image of a development. When applied, the following guidelines will enhance the sense of site continuity and contribute to a pleasant, orderly environment within the Route 31 Corridor.

- ▶ Lighting should be used to provide illumination for the security and safety of on-site areas such as parking, loading, shipping and receiving, pathways, and work areas. Maximum average light levels should not exceed 4.0 foot candles.



- ▶ All exterior lighting should be designed as part of the architectural and landscape design concept. The design of the light fixtures and their structural support should be architecturally compatible with the main building(s) on-site.
- ▶ Illuminators, fixtures, standards, and exposed accessories should be integrated within the building design.
- ▶ All lighting should be shielded to confine light spread within the site boundaries. In no case shall the illumination level beyond the property line exceed 0.5 foot candles.

- ▶ Adequate provisions should be made to avoid glare of vehicular and stationary lights that would affect the established character of the corridor, and to the extent such lights will be visible across any property line.
- ▶ Accent lighting that highlights a building's architectural features is encouraged. Exterior neon lighting, illuminated banding, or other lighting that creates a glow should be limited to accent purpose only.



- ▶ Building-mounted light fixtures should be for aesthetic and safety purposes only and must direct light upward or downward. Lighting should be used to highlight architectural features and create visual interest.
- ▶ Lighting that shines outward toward adjoining properties or street right(s)-of-way is prohibited.
- ▶ Metal halide lighting mounted at a 90-degree angle is the preferred method of lighting parking lots.



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- ▶ The preferred method of parking lot illumination is with individual light poles and fixtures. The use of building-mounted fixtures should be limited to special circumstances.



- ▶ To the maximum extent feasible, parking lot light poles/fixtures of the same style, height, color, and intensity of lighting should be used throughout the development corridor. Varying styles of fixtures may be permitted if it is demonstrated that the styles are compatible with and contribute to the overall lighting theme for the area.
- ▶ For parking areas, light pole and mounting heights should not exceed 25 feet above grade. Within 500 feet of Route 31 and residential properties, light poles shall not exceed 18'.

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## ❖ APPENDICES

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ENVIRONMENTAL FEATURES MAP

FRAMEWORK PLAN

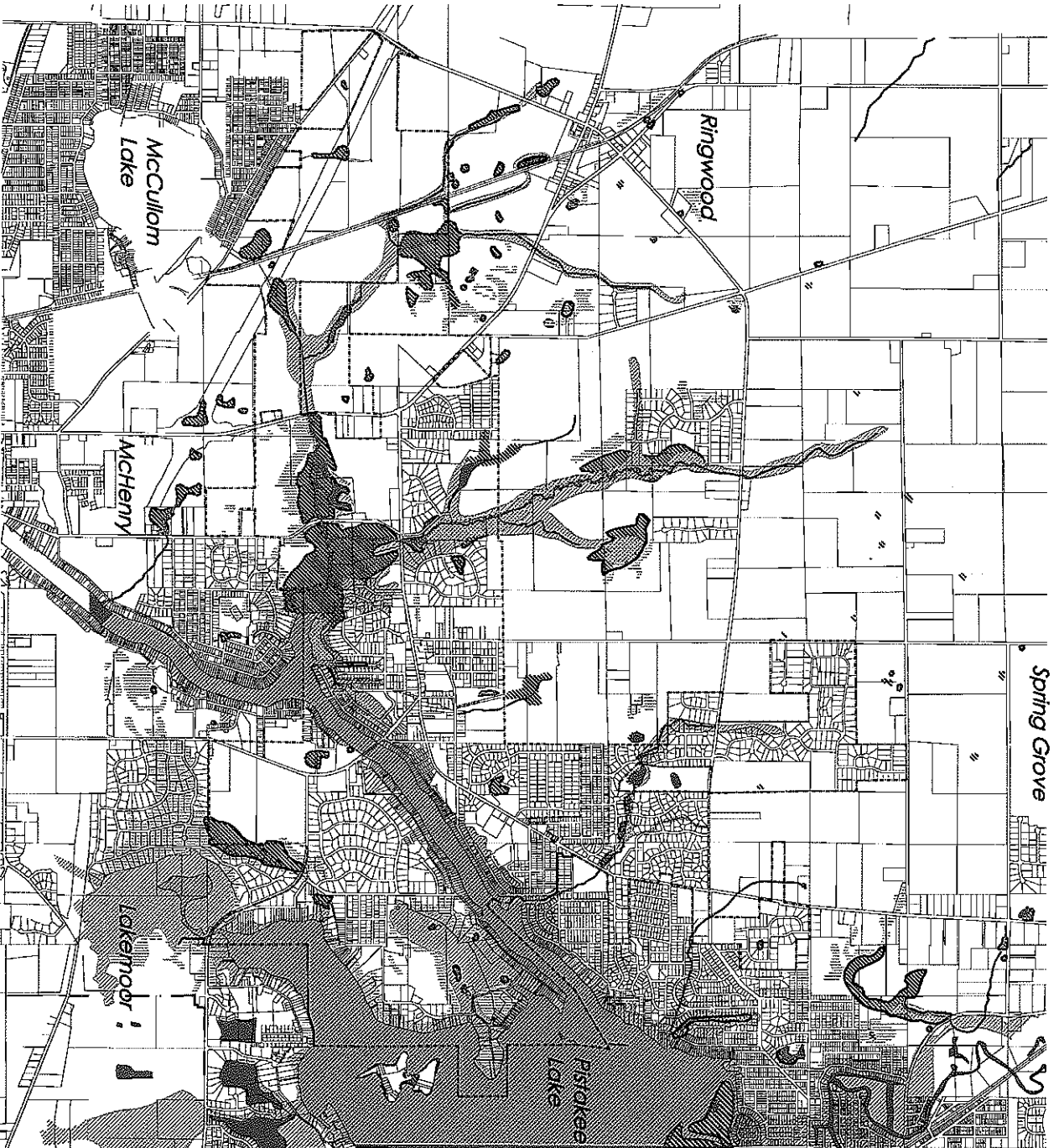
LAND USE CAPACITY TABLE

POPULATION/DEVELOPMENT ANALYSIS TABLE

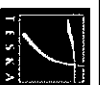
TRANSITIONAL ROAD DESIGN

# Environmental Features Map Village of Johnsburg

- General  
Johnsburg Village Units
- Environment  
Floodplain  
Wetland  
Poor Soil



December 2003



# **LAND USE CAPACITY ANALYSIS** **Village of Johnsburg - Comprehensive Land Use Plan** **December 2003**

Land Use Classification	Percent of Total Future	Future Gross Acres (approx.)	Floor Area Ratio		Total Square Footage		Density d.u./h.a.		Total Dwelling Units	
			(Low)	(High)	(Low)	(High)	(Low)	(High)	(Low)	(High)
Estate Residential	7.20%	632	-	-	-	-	0.10	0.50	47	237
Low Density Residential	36.45%	3,198	-	-	-	-	0.51	1.00	1,142	2,239
Medium Density Residential	28.32%	2,485	-	-	-	-	1.01	3.00	1,757	5,219
High Density Residential	2.92%	256	-	-	-	-	3.01	8.00	501	1,331
Commercial	8.21%	720	0.2	0.3	6,272,640	9,408,960	-	-	-	-
Office/Research	2.82%	247	0.2	0.3	2,151,864	3,227,796	-	-	-	-
Light Industrial/Business Park	1.57%	138	0.2	0.3	1,202,256	1,803,384	-	-	-	-
Historic Downtown	0.15%	13	1	1.5	566,280	849,420	-	-	-	-
Municipal/Public	1.40%	123	-	-	-	-	-	-	-	-
Open Space and Conservation	10.67%	936	-	-	-	-	-	-	-	-
Train Coach Yard	0.30%	26	-	-	-	-	-	-	-	-
<b>Total:</b>	<b>100.00%</b>	<b>8,774</b>			<b>10,198,040</b>	<b>15,289,560</b>			<b>3,447</b>	<b>9,025</b>

Land Use Classification	Total Population		Park Acres Required		Total Elementary Aged Children		Total Junior High Aged Children		Total High School Aged Children	
	(Low)	(High)	(Low)	(High)	(Low)	(High)	(Low)	(High)	(Low)	(High)
Estate Residential	178	892	2	9	25	126	14	71	64	321
Low Density Residential	4,297	8,426	43	84	605	1,186	340	667	1,547	3,033
Medium Density Residential	5,093	15,128	51	151	648	1,926	304	903	937	2,784
High Density Residential	1,198	3,184	12	32	117	312	29	77	71	188
Commercial	-	-	-	-	-	-	-	-	-	-
Office/Research	-	-	-	-	-	-	-	-	-	-
Light Industrial/Business Park	-	-	-	-	-	-	-	-	-	-
Historic Downtown	-	-	-	-	-	-	-	-	-	-
Municipal/Public	-	-	-	-	-	-	-	-	-	-
Train Coach Yard	-	-	-	-	-	-	-	-	-	-
<b>Total:</b>	<b>10,767</b>	<b>27,631</b>	<b>108</b>	<b>276</b>	<b>1,396</b>	<b>3,549</b>	<b>687</b>	<b>1,718</b>	<b>2,619</b>	<b>6,326</b>

Note:

Estimates for school age population based upon Illinois School Consulting Service/Associated Municipal Consultants, Inc. 1996 data

Total Elementary School Acres		Total Junior High School Acres		Total High School Acres	
(Low)	(High)	(Low)	(High)	(Low)	(High)
28	71	13	33	52	127

# Population/Development Analysis

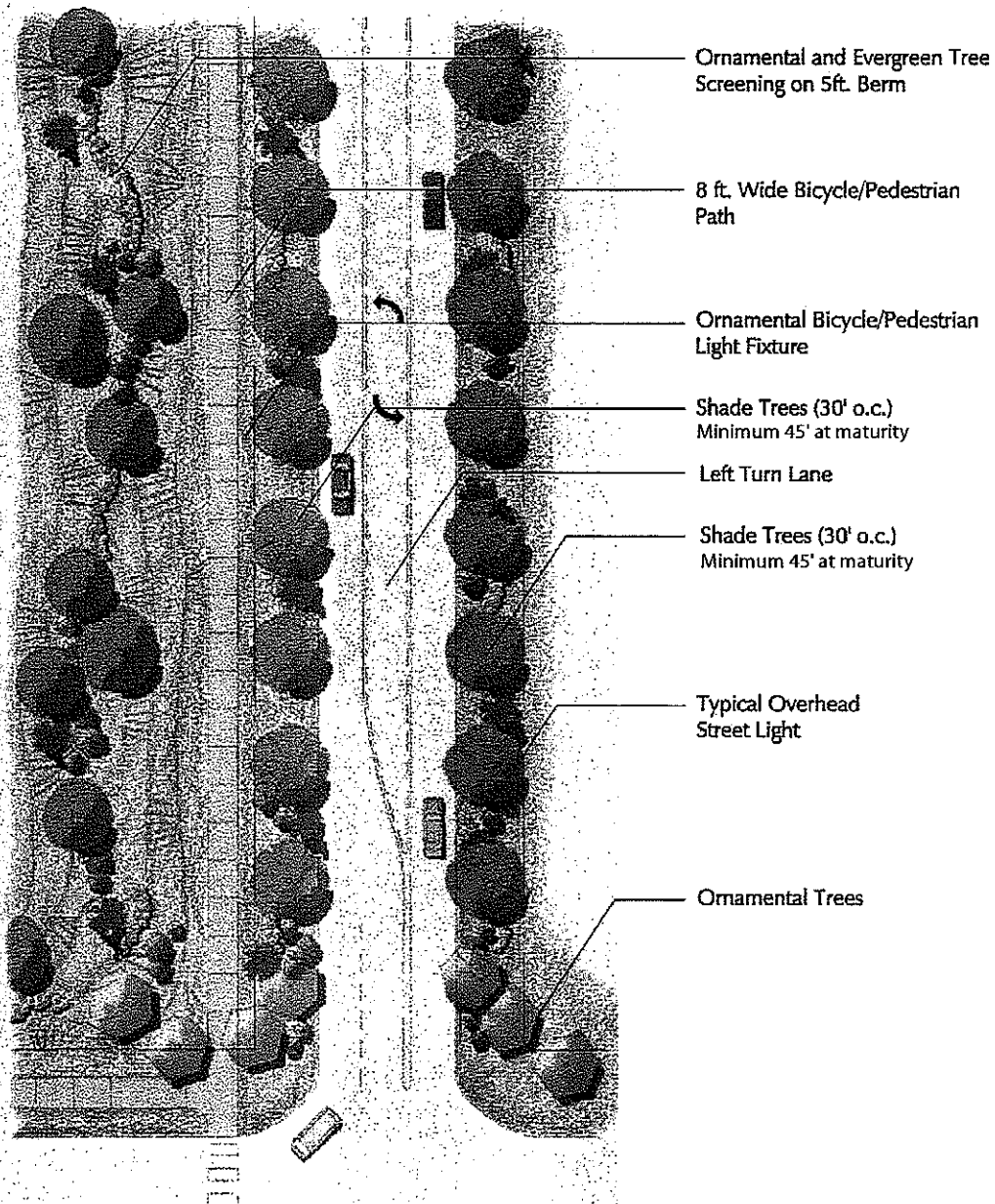
## Route 31 Planning Area (Planning Area 5)

### Village of Johnsburg - Population / Development Analysis

November 7, 2002

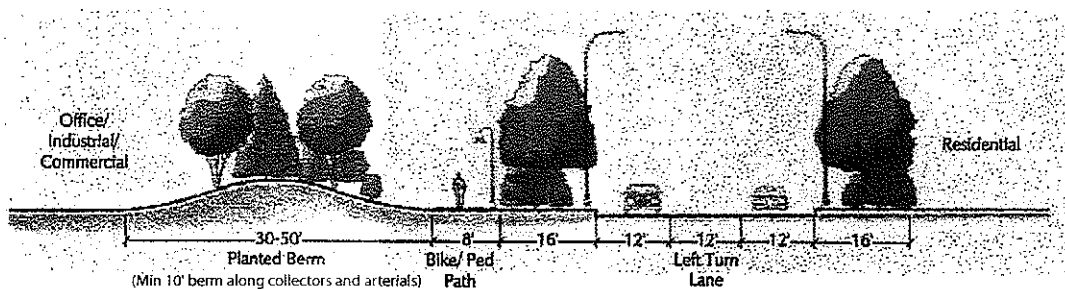
Prepared by: Timothy J. Hartnett - Smith Engineering

Parcel	Acres +/-	Comprehensive Plan Description	Commercial Use (Acres)	Residential Use (Acres)	Residential Type (Single Family or Multi Family)	Total Units	Density (Dwelling Units/Acre)	Avg. Population Generated (People)	School Children Generated		Project Buildout	Sales Rate / Year	Construction Start	Construction End	
									K-8	HS					
Windsor Dev. East	104.71	Medium Density P.U.D.	19.5	85.21	MF 21.97 Acres SF 63.24 Acres	144 108	6.55 1.7	4.12	360 378	52 41	15 12	3-5	50	Spring 2003	Fall 2006-8
Windsor Dev. West	73.43	Mixed Use P.U.D.	22.0	51.43	SF/MF Mix Assume 1/2 ea.	178	3.5		490	68	20	4-5	50	04 - 05	2008-2009
Gene Blake	12.4	Mixed Use P.U.D.	5.85	6.54	MF = 6.54 Acres	39	6		97	14	4	1-2	50	03 - 04	2004-2006
Anest Insigntia	204.9	Mix Use P.D.	*25.0 (12.5) 1/2 Useable	179.9	MF = 20 Acres SF = 159.90 Acres	89 142	4.45 1.1	2.79	222 497	32 54	9 16	5-7	50	Summer 2003	Fall 2008-10
Johnson	220.75	Mix Use P.D.	18.53	202.22	MF = 64.68 Acres SF = 137.54 Acres	388 224	6 1.62	3.81	970 784	140 85	39 25	8-10	50	03 - 04	2011-2014
Voss	245.2	Mix Use P.D.	57.42 B3 *40.0 Metra	147.73	MF = 62.78 Acres SF = 85 Acres	376 212	6 2.5	4.25	940 742	136 81	38 23	8-10	50	03 - 04	2011-2014
Baron	79.28	Light Industrial Business Park	25	54.28	MF = 15.5 Acres SF = 38.78 Acres	93 57	6 1.5	3.75	232 199	34 22	9 6	3-5	50	04 - 05	2007-2010
Pollara	85	Light Industrial Business Park	46.8	38.2	MF = 32.2	229	6		572	83	23	5-6	50	04 - 05	2009-2011
Slade	84.74	Commercial / Medium Density	32	52.74	MF = 52.74	158	3		395	57	16	4-6	35	05 - 06	2009-2012
Balmes	169.72	Commercial / Low Density	36.4	133.3	SF = 133.3	133	1		465	51	15	6-7	20	06 - 08	2012-2015
SUB-TOTAL A		1,280.13	276.00	951.55	MF = 308.12 SF = 643.47	1,605 965	5.2 DU/AC 1.5 DU/AC	3.35	7,343.00	950	270				
Estate Density	628				SF = 628	314	0.5		1,099	120	35	12-15	20	07 - 10	2019-2025
Low Density	1,274				SF = 1,274	1,274	1		4,459	484	140	*	30	02 - 10	*
Medium Density	107				SF = 107	321	3		1,123	122	35	8-10	40	04 - 06	2012-2016
Commercial Property Chapel Hill / Bay Road	56													05 - 06	
SUB-TOTAL B		2,065.00			2,009	1,909.00	.89 DU/AC	6,681.00	726	210					
GRAND TOTAL A+B		3,345.13	276.00	951.55	MF = 308.12 SF = 2,652.47	1,605 2,874			14,024.00	1676	480				



**Plan View of Transitional Road**

Not To Scale



**Typical Transitional Road Section**

Not To Scale